

# LLYNCH

## TRANSPORT SAFETY MANUAL

PUTTING SAFETY FIRST

**VERSION 4**  
FEB 23





## INTRODUCTION

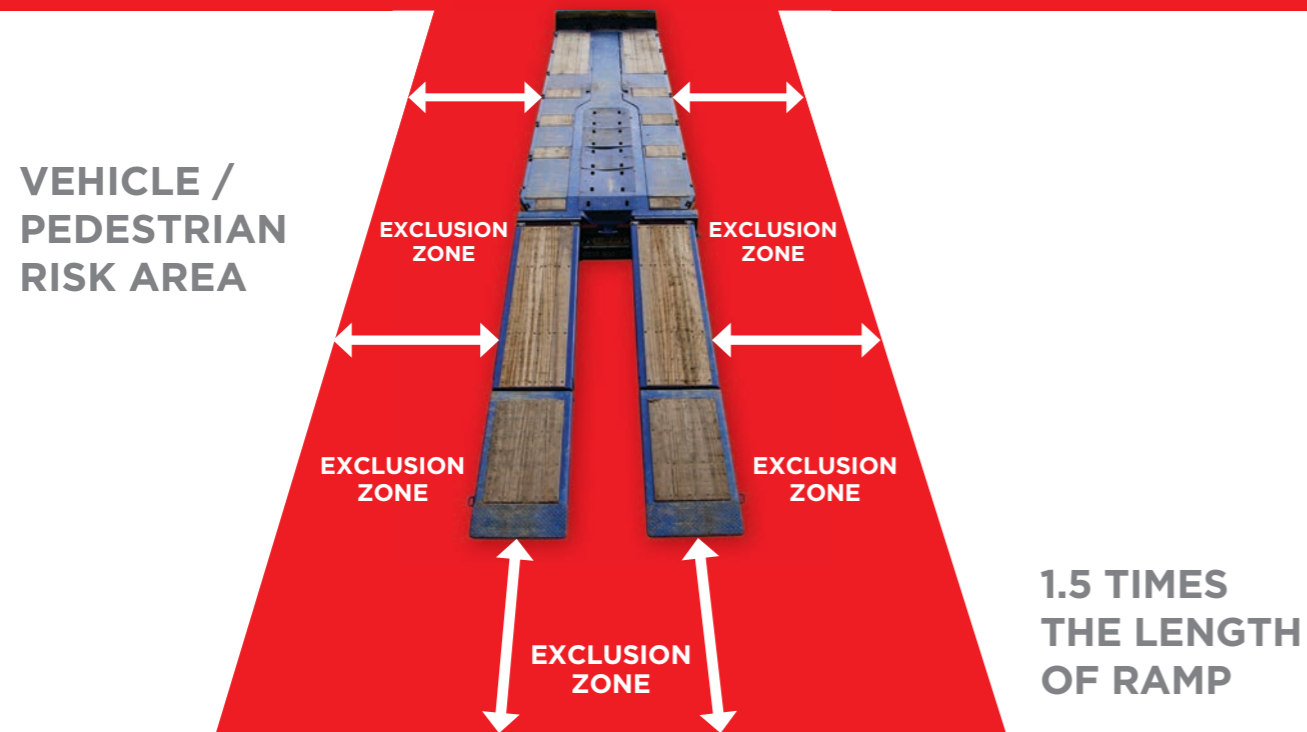
- We face a number of risks and hazards in our day to day activities in Plant Transport. This manual addresses these risks and sets out our procedures to follow for working safely at L Lynch Plant Hire & Haulage.
- This document will be updated as new works activities bring up further safety issues and to keep in line with new legislation.
- Carrying out your daily duties in a safe working environment is important in an ever-changing workplace.
- Your safety on site and when at Lynch depots is paramount in everything we do.
- Please use this safety manual to refer to in all cases. Should you need further assistance make sure you contact your line manager for guidance.
- Remember — **‘NEVER TAKE RISKS!’**

This Document does not replace the extensive guidance available in:  
 Department for Transport Code of Practice  
 The European best practice guidelines on cargo securing for road transport  
 Road Vehicles (Construction & Use) Regs 1986  
 Road Vehicles (Authorisation of Special Types) (General) order 2003 (STGO)

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## ➤ LOADING AND UNLOADING

### RISK ASSESSMENT

We have identified the following risks from this activity:

- Falls from height, people or objects from the transporter load bed and ramps
- Falls from height of vehicles drivers from the transporter load bed and ramps
- Slips & Trips (injuries sustained, sprains, abrasions, lacerations)
- Driver injuries from moving vehicle/trailer parts (ramps/jacklegs)
- Site personnel struck by moving vehicles and plant. (PPI)
- Electrocution from contact with overhead power cables

### PEOPLE AFFECTED BY THIS ACTIVITY

- Drivers
- Public and vulnerable road users
- Site Personnel

### SAFE SYSTEM OF WORK

The following Lynch Safe Working Procedures intends to eliminate or minimise the risks:

- 01 PPE required: High-vis Vest / Jacket, Safety Boots, Safety Gloves, Safety Helmet, Safety Glasses, (Site/Contract Specific) – ensure that you report to security, attend a site induction and adhere to site rules. If in doubt, contact your line manager.
- 02 Transporter driver must be competent to drive / move and load the plant item. The Transport Manager and training team ensures the competency of the driver.
- 03 All drivers must be familiar with the controls on any item of plant that they operate. All drivers must have received the correct information or instruction and training to do so.
- 04 The vehicle should be on firm and level ground while loading and unloading.
- 05 Check transporter bed is clear of debris and chains before working.
- 06 It is the drivers responsibility to select a safe area to load/unload away from overhead obstructions like power cables and away from excavations. This should be done with the co-operation of site supervisors / managers.
- 07 Keep all limbs clear of pinch points and crush zones on ramps and trailers when setting load bed to receive plant.

- 08** Plant with rollover protection should be operated with roll bar in **ON** position.
- 09** Seat belts to be worn at all times.
- 10** When collecting plant from site, check plant is free of major defects. **STOP** working if defects are found that may compromise safety.
- 11** When collecting plant from site, be aware that muddy or wet conditions may cause machines to slip on ramps. In these conditions and to avoid losing traction, tracks/wheels must be cleaned and/or sand be placed on the ramps. Bags of sand are available in the workshop. All vehicles are equipped with rubber anti-slip mats to assist the traction control and are to be used where necessary.  
  
Failure to adhere to this advice could lead to the machine sliding/falling from the lorry whilst loading. **IF IN DOUBT, PLEASE CONTACT YOUR LINE MANAGER BEFORE LOADING ANY MACHINE - NEVER TAKE RISKS.**
- 12** Ensure site workers and pedestrians are a safe distance away from the work area, particularly at the bottom of the ramps and to the sides of the lorry. Maintain an **EXCLUSION ZONE** at all times. If situations present themselves for persons to be around the lorry, then **NO MOVEMENT** will take place until supervised by the on site manager.
- 13** Do not jump off the transporter load bed. Walk down the ramps or sit down and lower yourself gently off the edge. Use manufacturer fitted steps where possible.
- 14** Do not rush what you are doing; the risk of an accident (particularly trips) is much higher.
- 15** All vehicles that are fitted with Fall Arrest – these must be deployed at all times when practicable.
- 16 ALL LOADED VEHICLES MUST BE MEASURED FOR HEIGHT AND CAB INDICATOR MUST REFLECT HEIGHT WHILST TRAVELLING.**



## ➤ VEHICLE / TRAILER BED

- 01** Sweep down the load bed after every delivery and collection. Fines could be incurred for an insecure load.
- 02** Clean down plant item before loading, ensuring that all loose debris has been removed.
- 03** Secure chains / straps in a safe place.
- 04** Tools must be stowed away correctly.
- 05** Report all defects to the workshops and your line manager and ensure you report on the relevant defect app.
- 06** Handrails to be fixed and secured at all times unless carrying oversized plant.
- 07** Ensure load restraints are in a serviceable condition.

## ➤ SECURING PLANT ITEMS TO THE TRANSPORTER

### RISKS IDENTIFIED

We have identified the following risks for this activity:

- Crush
- Trip
- Impact
- Plant fall from height (vehicle/trailer)
- Working at height
- Slips

### PEOPLE AFFECTED BY THIS ACTIVITY

- Drivers
- Public and vulnerable road users
- Site Personnel

### SAFE SYSTEM OF WORK

The following Lynch Safe Working Procedures intends to eliminate or minimise the risks:

- 01 PPE required: High-vis Vest / Jacket, Safety Boots, Safety Gloves, Safety Helmet, Safety Glasses, (Site/Contract Specific).
- 02 Use care when loosening ratchets, they can snap back, causing injury.
- 03 Keep area clear.
- 04 Exercise extreme caution when walking around the vehicle/trailer bed, looking out for potential trip hazards. Ensure fall arrest is deployed when practicable.
- 05 Use official guidelines on chain positioning.

### CHAIN POSITIONING FOR SECURING PLANT

- 01 All machines must be secured using the correct chains and/or straps rated to the weight of the machine, multiple chains/straps may be required. The Department of Transport code of practice for safety of loads and vehicles advises that we always follow manufactures recommendations when securing plant.
- 02 The following pages contain diagrams specific to individual machines, showing the correct way to secure plant and equipment.



## ➤ GENERAL GUIDANCE ON LOADING, UNLOADING AND SECURING PLANT

### ➤ LOAD SECURITY, THE LAW

This is covered by The Road Vehicles (Construction and Use) Regulations 1986 no 1078 and this states that;

“The load carried by a motor vehicle or trailer shall at all times be so secured, if necessary by physical restraint other than its own weight, and be in such a position, that neither danger nor nuisance is likely to be caused to any person or property by reason of the load or any part thereof falling or being blown from the vehicle or by reason of any movement of the load or any part thereof in relation to the vehicle.”

- 01 When loading excavators the operator must face the direction of travel at all times where possible.
- 02 All dumpers and rollers under 3 ton are to be reversed onto the vehicle/trailer with Roll Over Protection Systems (ROPS) engaged, seat belt fastened and driven off where possible.
- 03 All bobcats are to be reversed onto the vehicle/trailer and driven off where possible.

- 04 All drivers are to be aware of machine weights and their positioning on vehicles/trailers as not to overload any of the axles.
- 05 Where possible, all forklifts should be driven on and reversed off due to their heavy counter weight.
- 06 Drivers carrying large machines must have arms folded completely and be aware of boom height, so to minimise risk of bridge strikes etc. All drivers must carry out height measurements prior to departure and display height in cab.
- 07 All machines regardless of weight should be secured using manufacturers guidelines. These need to be adequate enough to stop 100% of the weight of the machine moving forward, 100% moving rearward and 50% to the side.
- 08 When carrying multiple machines, drivers must arrange the plant in such a manner that does cause damage to any item.
- 09 Drivers must be aware of any overhead obstructions particularly overhead power lines and low telegraph cables when loading / unloading machines. Do NOT work within 9 metres horizontally of overhead power cables supported on wooden poles. DO NOT work within 15 metres horizontally of overhead power cables supported on steel pylons or structures.
- 10 Drivers of Hiab vehicles are to ensure cranes are always stowed away before moving off and crane and stabiliser legs are stowed correctly. All Warning indicator/Alarms must be fully operational. Any defects must be reported on the aquarius reporting app.
- 11 All vehicles are to be washed weekly as a minimum - more often in Winter if dirty.
- 12 Drivers must be aware of the danger zone and pinch points on the ramps of transporters. Exclusion zones must be adhered to at all times.
- 13 It is the responsibility of the driver to load, unload and secure machines on their vehicles/trailers.
- 14 Attachments should be loaded using the correct method as per risk assessment, method statement,
- 15 Safe System of Work (SSOW). Only one attachment per Quick Hitch. **BUCKETS MUST NOT BE DOUBLE HITCHED OR CARRIED ON TEETH.**

## ➤ WARNING

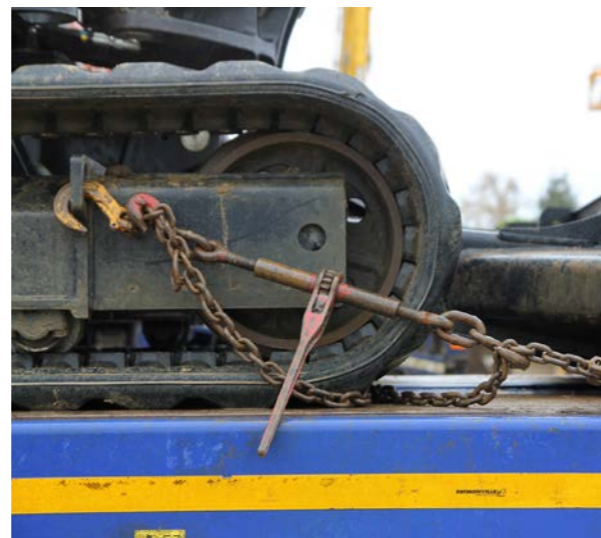
- The safe transit of the load is the responsibility of the driver. Any machines attachments and parts must be secured.
- Drivers must ensure their vehicle is the correct vehicle to transport the load and that they have the correct load restraint equipment.

## ➤ WARNING

- Ensure the vehicle ramps are clean and free from debris, grease, oil and ice. This includes checking the Plant item to be loaded - tyres free from oil/ grease etc.
- Check ramp pins are not damaged, Ramps are lowered correctly, and support legs deployed; ensuring the exclusion zone is adhered to.

## ➤ OVERLOADING

- It is the driver's responsibility to ensure that the vehicle/trailer is not overloaded. Check the chassis plate for maximum weight limits. Both the driver and L Lynch Plant Hire and Haulage are liable to prosecution for overloading, which could result in a fine and endorsements for operator and driver.

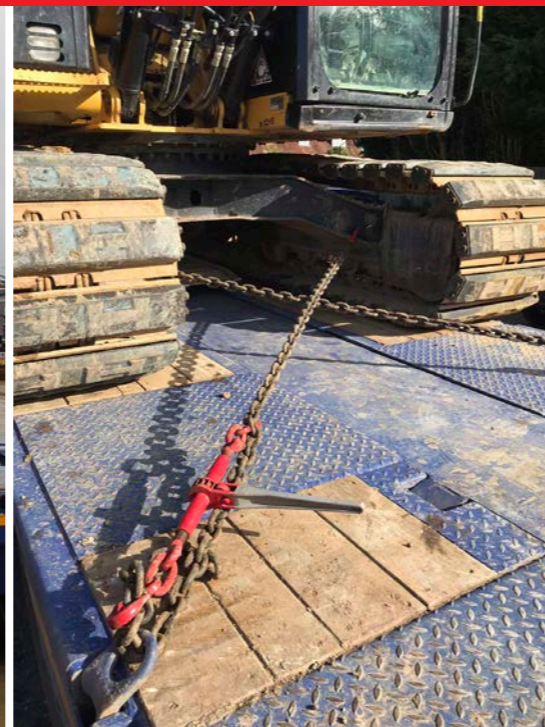


## ➤ CORRECT RESTRAINT POSITIONING FOR AN EXCAVATOR

- STEP 1:** Secure the machine to the load bed.
- STEP 2:** Fasten the undercarriage to the load bed using chains at four positions A (two each side). Ensure correct chains are used.
- STEP 3:** Secure the arm to the load bed using either the correct hook/strap/shackle or link as appropriate (item B).
- STEP 4:** Measure the highest part of the machine from the ground. It is the driver's responsibility to ensure that before moving away, the in cab height indicator is changed.
- STEP 5:** Unloading the machine from the load bed.



**CORRECT CHAIN POSITIONING FOR EXCAVATORS**



**SECURE THE LOCKING BAR FOR TRANSIT ON EXCAVATORS WHEN FITTED**

## ➤ CORRECT RESTRAINT POSITIONING FOR EXCAVATORS

After loading the machine secure as described below.

- 01 Lower the dozer blade (if fitted).
- 02 Fully crowd the quick hitch retract the dipper arm and lower the main arm lowering to the load bed.
- 03 Stop the engine and release any residual pressure in the system before removing the ignition key.
- 04 Ensure the plant control safety lever inside the cab is in the locked position.
- 05 Place chocks/blocks in front and behind the crawlers; if necessary.
- 06 Cross chain all machines where required.

- 07 Secure all buckets and attachments with a chain or ratchet straps.
- 08 All machines regardless of weight must be secured using manufacturers guidelines.
- 09 Rubber tracked machines can be strapped over the tracks or chained over the H frame. **UNDER NO CIRCUMSTANCES SHOULD CHAINS BE USED OVER RUBBER TRACKS.** Where possible place a wooden block under the bucket so that it is not damaged by hitting the floor.
- 10 Secure the locking bar for transit on excavators when fitted.

## ➤ WARNING

- Always consider the length, width, height and weight of the truck with the machine loaded on it to determine the best route. If travelling under STGO regulations the movement order must always be adhered to.



## ➤ CORRECT CHAIN POSITIONING FOR ARTICULATED DUMPTRUCKS

### LIFTING THE DUMPER

- When lifting dumpers appropriate lifting straps must be used, these lifting straps must be fed through the handle at the edge of the dump body. When lifting, slew in the articulation block and secure it by means of a spring cotter.

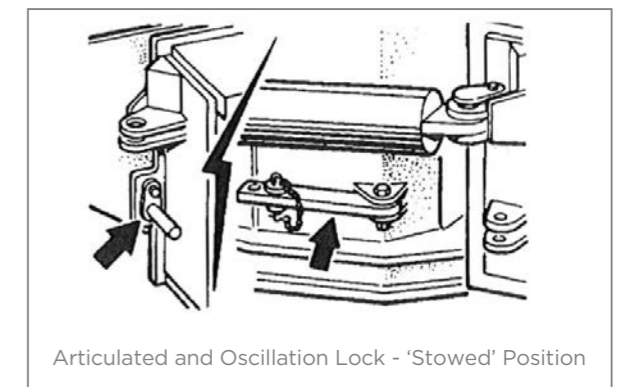
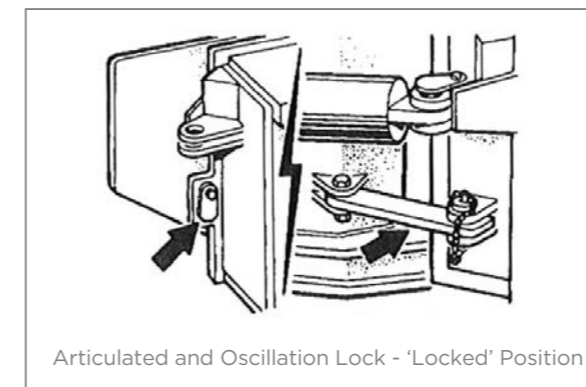
### LOADING AND TRANSPORTATION

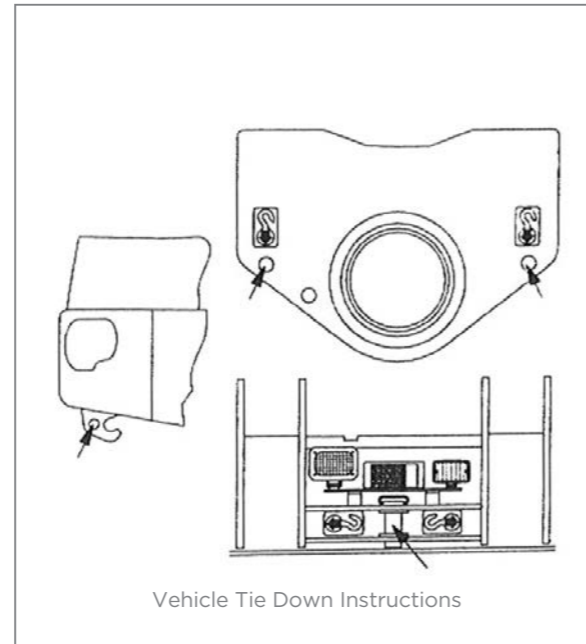
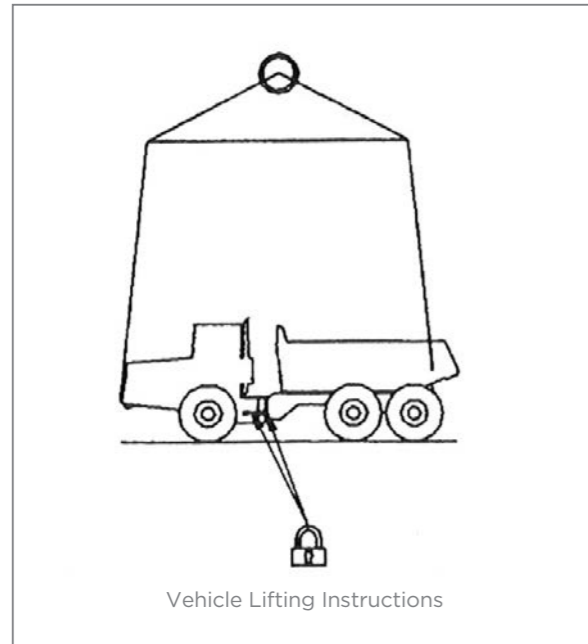
- Drive the unloaded dumper in slow reverse motion via a loading platform onto the load bed, or load it by means of a crane (see 'Lifting the Dumper' above).
- The dumper should be secured in accordance with manufacturers guidelines.
- All plant must be loaded using lowest revs/gear.

## ➤ ARTICULATED DUMPTRUCKS

### ARTICULATION AND OSCILLATION LOCK

- Always connect the steering and oscillation lock bar before working in the articulation area, before lifting the vehicle and during transportation of this vehicle by trailer/vessel.
- Always disconnect the steering and oscillation lock bar and secure in the 'Stowed' position before operating the vehicle. The vehicle would not be free to steer otherwise.





#### VEHICLE LIFTING PRECAUTIONS

- Prior to any lifting, vehicles must be parked on a level surface, wheels blocked, steering and oscillation lock bar connected, and the parking brake disengaged.
- Vehicles must be lifted using a spreader bar if possible. Lift using FOUR slings from the lifting points provided at the bumper end of the front chassis and at the rear of the body.

#### VEHICLE TIE DOWN PRECAUTIONS

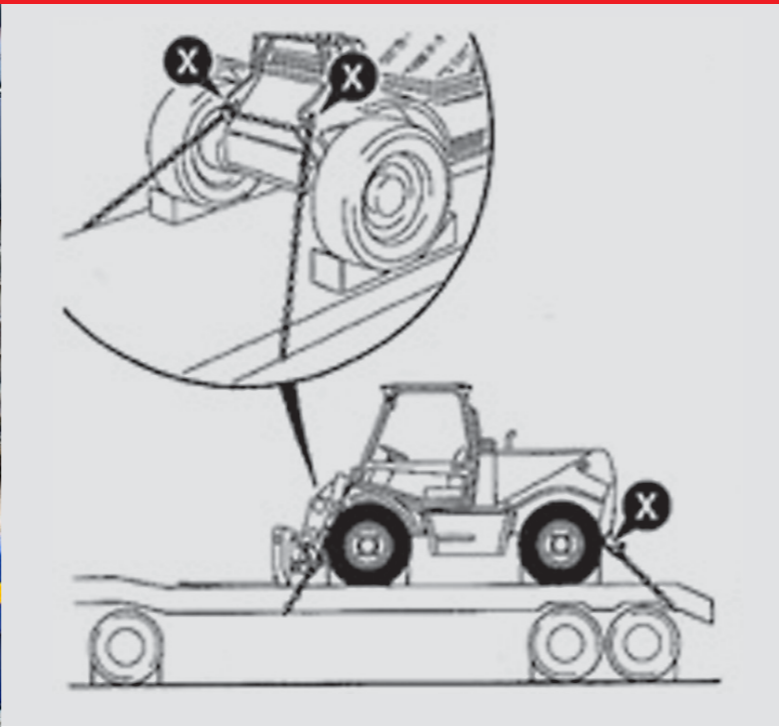
- The vehicle must be secured at the tie down points located at the bumper end of the front chassis, the front face of the trailer chassis and the tow pin at the rear of the trailer chassis.

**100% Forward**  
**100% Rear**  
**50% N/S +O/S**

## ➤ TELEHANDLERS

Load the machine onto a load bed as follows:

- 01 Move the machine onto the load bed.
- 02 Ensure the ramps are correctly in place and secured, where applicable.
- 03 Lower and secure the boom ready for transporting.
- 04 Carefully load the machine onto the load bed.
- 05 Engage the park brake and set the drive to neutral.
- 06 Lower the forks onto the load bed.
- 07 Secure the stabilisers in the raised position.



- 08 Check that the overall height of the load is within regulations. Adjust if necessary.
- 09 Switch off the engine and release any residual pressure in the system before removing the ignition key.
- 10 Secure the cab.
- 11 Place chocks/blocks at the front and rear of all four tyres if required.
- 12 Secure the machine to the load bed with chains.

**WARNING:**

- The safe transit of the load is the responsibility of the transport contractor and driver. Any machine attachments or parts that may move during transit must be adequately secured.
- If travelling under STGO regulations the movement order must be adhered to at all times.
- Ensure the transporting vehicle is suitable and that the chains/straps used are of the correct rating for the weight of the transported machine.

**WARNING:**

- Before moving the machine onto the load bed ensure that the load bed and ramp are free from oil, grease and ice. Remove oil, grease and ice from the machine tyres. Ensure the machine will not fail on the ramp angle.



## ▶ TRACTOR & BOWSER/TRAILER

### LOADING/UNLOADING THE TRACTOR & BOWSER/TRAILER

- 01 Set load bed and ramps for the operation to take place.
- 02 Ensure that the vehicle load bed and ramps are free of debris, mud, oil, grease, waste, ice and other possible slip/trip hazards. (Sand to be spread where necessary to increase traction).
- 03 Seatbelt to be worn at all times when inside the cab.
- 04 Thorough observations are to be made before and during the loading activity.
- 05 Ensure that the machine is central to the load bed as possible.
- 06 Use banksman where possible when loading/unloading. Agree lines of communication prior to loading/unloading commencing. If you lose sights of the banksman at any point, stop until contact is re-established.
- 07 Select forward gear, travel the machine up the ramps at a speed suitable for the operation. DO NOT RUSH.
- 08 When the machine has reached its suitable loading position, select neutral and park brake.

- 09 Exit machine using handholds and footholds provided, maintaining 3 points of contact at all times.
- 10 If any part of the loading/unloading of plant falls outside of the above at any time or the operation does not go as planned STOP IMMEDIATELY and report to site Black/Grey Hat and Lynch Transport Office DO NOT CONTINUE until a recovery solution has been agreed by Lynch Transport Office and customer.

### CORRECT RESTRAINT POSITIONING FOR A TRACTOR & BOWSER/TRAILER

- 01 Secure both the tractor & trailer/bowser as individual items.
- 02 Secure the machine to the load bed.
- 03 Fasten the undercarriage to the load bed using chains at four positions (two each side). Ensure correct chains are used.
- 04 Strap wheels if there are no suitable manufacturers lashing points. Under no circumstances are chains to be used over tyres/wheels – use straps.
- 05 Place chocks where required.

## ▶ WARNING:

- The safe transit of the load is the responsibility of the transport contractor and driver. Any machine attachments or parts that may move during transit must be adequately secured.
- If travelling under STGO regulations the movement order must be adhered to at all times.
- Ensure the transporting vehicle is suitable and that the chains/straps used are of the correct rating for the weight of the transported machine.

## ▶ WARNING:

- Before moving the machine onto the load bed ensure that the load bed and ramp are free from oil, grease and ice. Remove oil, grease and ice from the machine tyres. Ensure the machine will not fail on the ramp angle.



## ➤ TRANSPORT DRIVER AND DELIVERY PROCEDURES

- It is the driver's responsibility to check they have the right machines and attachments etc. Ensuring that it is secured correctly.
- If the machine that the driver is delivering has additions that aren't on the syrx job (e.g. extra buckets) please write these in the comments and inform the office who will amend the job accordingly.
- When delivering or collecting plant, if the driver is held up on site exceeding 20 minutes, then they must call their line manager or the office for assistance.
- Syrx jobs must be filled out for all deliveries and collections. Clear photos must be taken to include the whole of the machine diagonally from each corner, with the machine number clearly visible; all buckets and attachments must also be photographed, when they are numbered this must be shown clearly. As many photos as is necessary should be taken with all damage recorded clearly both via photo and noted in the comments section as well as recorded behind the individual item. Fuel must be checked and signed off by the customer on site with photos taken. If there is no one on site the driver must phone the Hire Team who will inform the customer.

If there are any shortages regarding the collection, eg buckets and attachments please contact the office before leaving the site so they can establish whether they have been stolen or misplaced.

- All damages **MUST** be recorded on the PDA with photos taken. If the machine collected is to be kept on hire inform the office immediately, stating the reason why the customer would like to keep the machine. If the machine cannot be collected for any other reason, e.g. no keys, broken down etc., **YOU MUST** inform the office immediately and not leave site until given permission to do so.
- All vehicles should be fuelled at the end of every shift unless the tank is already full. All drivers must sign in and out daily by use of the fob. All trucks will have fuel fobs. These must stay with each truck. If your truck doesn't have a fuel fob then speak to your manager/supervisor but do not swap the fobs as this interferes with the mileage for the original vehicle allocated to that fob.
- When completing machine moves or day work, paperwork/PDA **MUST** be signed at the end of each day - failure to do this may result in the company invoices being delayed and not being paid on time. Time on and off customer sites must be written down for all those jobs.



## ➤ USE OF CRANE LORRY HAZARD & RISK

### HAZARD ASSESSMENT

The following hazards have been identified in this activity:

- Difficult or awkward loads.
- Uneven ground.
- Close proximity of pedestrians.
- Close proximity of other vehicles or machines.
- Maintenance of vehicle crane and slings.
- Falls from height.
- Tools or materials falling from a working platform, containers or the structure.
- Movement of vehicles or cranes.
- Electricity.

### RISK ASSESSMENT

We have identified the following risks from this activity:

- Overturning of vehicle when carrying awkward loads.
- Overturning of vehicle on uneven or potholed ground.
- Personal injury from collision between vehicle and pedestrian.
- Personal injury and damage from collision between particular vehicle and other site vehicles.
- Failure of lifting equipment.
- Personal injury by falls from height.
- Being struck by falling tools containers or material.
- Being struck or crushed by overhead structures or piping.
- Electrical shock through contact with electrical units or overhead utilities.

## ➤ USE OF CRANE LORRY

### SAFE SYSTEM OF WORK

The following Lynch Safe Working Procedures are designed to eliminate or reduce the risks:

- 01 Only trained certificated employees are authorised to drive and operate site specified HIAB vehicles.
- 02 Correct PPE must be worn at all times as set out according to Lynch and site specific rules.
- 03 Warning beacons fitted to vehicles.
- 04 Reversing bleeper fitted to vehicle.
- 05 Daily defect checks must be carried out by the driver, documented and reported to workshop and line manager.
- 06 Mechanical maintenance is to be carried out only by authorised personnel.
- 07 Drivers are to ensure that their crane is adequately greased at all times.

- 08 Thorough examination conducted on vehicle and crane lifting accessories in accordance with LOLER 1998.
- 09 All Lynch transport drivers must report to site security/office on arrival at any customer site and wait further instruction before continuing (Site Escort may be required).
- 10 First Aid information and emergency procedures to be given to driver by site on arrival.
- 11 Site manager or supervisor must be liaised with and all SSOW method statements and agreed lift plans being in place before any work is started.
- 12 Work area to be cordoned off and pedestrian, plant and vehicle traffic diverted away from the work area (cones or safety tape and signage).
- 13 If any overhead power cables are present, power must be isolated to ensure that all power is off before work commences.
- 14 Any loose items that are on top of or inside the load to be lifted, will be removed or secured before lift.
- 15 All tools and other items inside containers to be cleared or secured by site before commencing work.
- 16 All works must be managed, supervised and instructed by site; any new hazards identified or that may occur on the day work commences must be risk assessed by site before any work proceeds and notified to persons who perform the tasks.
- 17 Crane support legs must be fully deployed before any lift can commence.
- 18 If crane support legs cannot be deployed due to site restrictions, the appointed person and your line manager must be informed.



## ➤ DRIVING SAFELY

- The penalties to the driver, the company and the general public of not driving safely can range from just being inconvenient to very serious and, sometimes catastrophic.
- As the driver of a heavy goods vehicle, the driver has a responsibility – not just to themselves but to all other road users. The driver can set a good example to others by driving safely, courteously and with consideration to all other road users.
- Employers will take great care to ensure that the vehicles their employees operate are safe and maintained to the appropriate legal standard. Vehicles will be maintained to the manufacturer's guidelines and in line with the legal requirements set out by the Traffic Commissioner. It is the drivers responsibility to:
  - Check their vehicle regularly / daily walk round checks.
  - Know the route they are driving.
  - Be aware of weather forecasts.
  - Drive safely.
- Many accidents are caused by driver error and the majority are preventable. Therefore it is important that all drivers take time to assess potential hazards and dangers that they could possibly encounter on their journey.
- Ensure all drivers plan their routes when oversized loads are being carried. If travelling under STGO regulation the movement order must be adhered to at all times with the STGO board correctly displayed at all times.



## ➤ DRIVING SAFELY: BASIC PRINCIPLES

### CAMERAS/MIRRORS

- It is important to know as much about the traffic and obstacles around as possible. Before starting any journey drivers must ensure all cameras and display screens are in good working order, as well as checking mirrors are damage free and are in the correct position for the driver to see around their vehicle. Drivers must make full use of their cameras/mirrors before making any manoeuvre.
- Drivers must be aware of any blind spots for themselves and others. Many larger goods vehicles are fitted with 'close proximity' and 'wide angle' mirrors to survey the driver's blind spot on the passenger side of the vehicle. It is particularly important that these mirrors are properly adjusted so that the driver can see (for example) cyclists close to the nearside of the vehicle. A defensive space must be kept around the vehicle to enable the driver to react if required.
- Drivers should use their cameras/mirrors frequently so that they are constantly aware of what is happening around them. Many vehicles are also fitted with direct vision standard windows in the passenger door; drivers must include this in their mirror checks.

### ROUTE PLANNING

- Route planning is an important part of any journey. The length, width, weight and height of a driver's vehicle will sometimes dictate the route they take.
- Before starting a journey, plan the route to include rest and identify any potential hazards.
- Planning the journey will help the driver select the safest and most efficient route. Drivers should always plan an alternative route to allow for accidents or bad weather conditions, unless travelling under STGO regulations, in which case the movement order should not be deviated from.

### FORWARD PLANNING

- Drivers to use the enhanced visibility provided by the high seating position in a cab to their advantage. Drivers should plan ahead for every manoeuvre they need to make, understand the road and consider the other road users around their vehicle.

### ROAD SPEED LIMITS

- **SPEED LIMITS SHOULD BE ADHERED TO AT ALL TIMES** for all relevant weight categories.

## ➤ DEPARTMENT OF TRANSPORT CODE OF PRACTICE – SAFETY OF LOADS ON VEHICLES

### 13.1

This section provides guidance on the measures necessary for the safe carriage of tracked and wheeled engineering plant by vehicles constructed to comply fully with the Motor Vehicles (Construction and Use) Regulations 1986 and thereby permitted unrestricted use of the roads. It does not deal with the carriage of large machines etc. on special purpose vehicles whose use on the roads is restricted by current regulations.

However, the general advice contained in this section will apply in many cases.

### 13.2

It is recommended that manufacturers of plant equipment either fit lashing points or provide a recommended lashing scheme for each of their vehicles.

### 13.3

Heavy engineering plant is normally transported on purpose built vehicles which are specifically designed to provide easy loading and unloading facilities and are usually provided with adequate anchorage points for attaching the lashings. Lighter engineering plant may in some circumstances be carried on general-purpose vehicles. However, in these cases the method used to secure the load should provide equal security to that obtained by using purpose built vehicles.

### 13.4

High loads may endanger bridges etc. over roads, so when these are carried it is essential that the driver know the exact height of the load, and the width of the load at that height. Also, loads with a high centre of gravity can seriously affect the vehicle's stability and such items of engineering plant should only be transported on vehicles with a low platform height.

### 13.5

A wheeled or tracked vehicle must be lashed down in position on the carrying vehicle, with the parking brake applied. The effectiveness of the parking brake on its own will be limited by the frictional resistance between the vehicle and the deck of the carrying vehicle, and even in normal driving conditions this will be inadequate and the vehicle will therefore require additional restraint. This additional restraint should take the form of a lashing system and some arrangement whereby the load is prevented from moving either forward or to the rear by an obstacle (or obstacles) securely fixed to the vehicle. These should butt against the wheels or tracks or some other part of the equipment carried.

### 13.6

Engineering plant should be dismantled as far as is necessary to keep its overall dimensions within the length, width and height (see para. 3.5) limits of the carrying vehicle. Where this is not possible, the conditions and restrictions contained in Regulations 10, 81 and 82 of the Motor Vehicles (Construction and Use) Regulations 1986: SI 1986 No 1078, and the relevant provision of the Motor Vehicles (Authorisation of Special Types) General Order 1979: SI 1979 No 1198, concerning the carriage of wide or long loads should be complied with.

### 13.7

All movable assemblies such as jibs, brackets, booms and cabs etc. must be left in the position recommended for transportation by the manufacturer and must be secured to prevent movement relative to the main body of the machine.

### 13.8

Before the machine is moved onto the trailer all loose dirt that may otherwise come off and obstruct the highway or damage other vehicles must be removed. The ramp, the tyres of the machine and the bed of the trailer itself should all be free from oil, grease, ice etc. so that the machinery cannot slip. The transporter wheels should be chocked at the front and rear.

### 13.9

When the machine has been stowed and the engine stopped, the pressure in the hydraulic system should be relieved by moving all of the control levers through all their positions.

This operation should be done at least twice. Controls should be set so as to prevent movement of ancillary items during transit.

### 13.10

Bags, tool kits, or other heavy objects should not be left loose in the operator's cab of the plant being carried.

### 13.11

The positioning of the engineering plant and any of its detached assemblies should be arranged so that the legal axle weight limits are not exceeded and the safe handling of the vehicle is not impaired. The clearance between the undersides of low loading vehicles and the road surface should be checked before moving off (see Section 3.6).

### 13.12

The machine should be positioned on the carrying vehicle's platform so that forward movement is prevented either by part of the main body of the vehicle, e.g. swan neck, step or headboard, or by an attached transverse member securely attached through the platform to the vehicle's chassis frame.

**13.13**

All items removed from the machine such as buckets, grabs, blades, shovels and lifting appliances should be lashed to the deck of the vehicle.

**13.14**

Wheeled and light tracked machines should be restrained so that the effect of bouncing caused by road shocks transmitted from the carrying vehicle and amplified by the machine's tyres or suspension units is minimised. Where possible the suspension unit of the machine should be locked and vertical movement limited by lashings or other means of restraint. Otherwise the machine's frame or chassis should be supported on blocks.

**13.15**

Unless the machine is supported, the full contact area of its tyres, tracks, or drums should rest on the deck of the carrying vehicle. If the tracks extend outside the frame of the carrying vehicle then the machine's frame or chassis should be supported.

**13.16**

The machine should be restrained against forward, backward and sideways movement by chain or webbing lashings attached to anchorage points on the vehicle. All lashings should incorporate some form of tensioning device.

**13.17**

In deciding the number of anchorage points to be used when arranging a restraint system, the following factors should be considered: The need to position the machine to achieve the correct load distribution to meet the legal axle load requirements and to ensure that the vehicle's handling is not impaired.

The extent to which other load restraint features is incorporated in the design of the vehicle.

Whether the machine has wheels, tracks or rollers.

The weight of the machine to be carried.

A minimum of four separate anchorage points should be used.

**13.18**

The following guidelines apply to mobile engineering plant, that is vehicles fitted with hoists, working platforms, support legs and so on.

1. High loads may endanger bridges and it is essential that the driver knows the height of the vehicle and has it displayed inside the vehicle cab.
2. All movable assemblies must be placed in the position and locked, where possible, as recommended for transportation by their manufacturer.

**RESTRAINING DEVICES****13.19**

Apart from specialised fixing devices, the selection of materials for use in tie down schemes for engineering plant will be limited to chains, steel wire rope, webbing and their associated tensioning and coupling devices.

**13.20**

Where a transverse (side to side) beam is used as a baulk it should be securely fixed so that all loads imposed on it are transmitted to the carrying vehicle's chassis frame. Where individual wheels or drums are chocked with blocks or scotches these must be robust enough to resist crushing and be securely attached to the vehicle's platform where possible.

**13.21**

The lashings or securing devices should only be attached to those parts of the engineering plant, which are of sufficient strength to withstand the stresses likely to be imposed on them.

**13.22**

Where engineering plant comes equipped with dedicated lashing points for use when being transported, these points should be used and the vehicle secured as per manufacturer instructions. Care should be taken before lashing to lifting points as these may not be suitable for restraint purposes.

**13.23**

The loaded machine should be inspected after the vehicle has been driven for a short distance in order to check that no movement has taken place and that restraining devices are fully secure. Periodic inspections should be made during the course of the journey.

## ➤ HEIGHT INDICATORS

Read in feet across and inches down – eg 13ft 7in = 4.14 metres

		FEET										
		10ft	11ft	12ft	13ft	14ft	15ft	16ft	17ft	18ft	19ft	20ft
INCHES	0in	3.05m	3.35m	3.66m	3.96m	4.27m	4.57m	4.88m	5.18m	5.49m	5.79m	6.10m
	1in	3.08m	3.38m	3.69m	3.99m	4.30m	4.60m	4.91m	5.21m	5.52m	5.82m	6.13m
	2in	3.10m	3.40m	3.71m	4.01m	4.32m	4.62m	4.93m	5.23m	5.54m	5.84m	6.15m
	3in	3.12m	3.43m	3.73m	4.04m	4.34m	4.65m	4.95m	5.26m	5.56m	5.87m	6.17m
	4in	3.15m	3.45m	3.76m	4.06m	4.37m	4.67m	4.98m	5.28m	5.59m	5.89m	6.20m
	5in	3.18m	3.48m	3.79m	4.09m	4.40m	4.70m	5.01m	5.31m	5.62m	5.92m	6.23m
	6in	3.20m	3.51m	3.81m	4.11m	4.42m	4.72m	5.03m	5.33m	5.64m	5.94m	6.25m
	7in	3.23m	3.53m	3.84m	4.14m	4.45m	4.75m	5.06m	5.36m	5.67m	5.97m	6.28m
	8in	3.25m	3.55m	3.86m	4.16m	4.47m	4.77m	5.08m	5.38m	5.69m	5.99m	6.30m
	9in	3.28m	3.58m	3.89m	4.19m	4.50m	4.80m	5.11m	5.41m	5.72m	6.02m	6.32m
	10in	3.30m	3.60m	3.91m	4.21m	4.52m	4.82m	5.13m	5.43m	5.74m	6.04m	6.35m
	11in	3.33m	3.63m	3.94m	4.24m	4.55m	4.85m	5.16m	5.46m	5.77m	6.07m	6.38m



- Ensure that the highest point of the load is measured and recorded.
- This must be done whenever the load is changed.
- Ensure that you are parked on firm level ground.
- Ensure that the vehicle/trailer air suspension has been levelled accordingly to the correct height.
- Using the issued height indicator measuring stick, extend each section until the highest part of the load is reached.
- The in-cab height indicator must be changed to reflect the highest part of the transporter and load.



## ▶ PLANT BODY SAFETY

### BASIC REQUIREMENTS

- 01** Keep yourself and all personnel clear of the load.
- 02** Keep yourself and all personnel clear of area behind loading ramp.
- 03** Keep yourself and all personnel clear of moving ramp.
- 04** Unlock loading ramp.
- 05** Do not move vehicle unless ramp is fully stowed and locking stays engaged.
- 06** Ensure machine being loaded/unloaded cannot roll away, chock if necessary.
- 07** Do not ride on machine being loaded/unloaded unless it technically requires driving.
- 08** Do not allow yourself or other personnel to be positioned between load and body headboard.
- 09** Manufactures guidelines should be adhered to at all times when securing machines.
- 10** Always ensure that the load bed is set correctly before loading/unloading.
- 11** Ensure suspension is restored to 'ride' position before moving vehicle.
- 12** Always ensure that there is adequate room for the operation to minimise risks.
- 13** If jacklegs are fitted to vehicle they must always be in firm contact with ground whilst loading. If the ground is too uneven for this then the jacklegs must be blocked to ensure they are supported.

## ▶ WINCH SAFETY

### BASIC REQUIREMENTS

- 01** Keep yourself and all personnel clear of the winch rope.
- 02** Never step over, stand near or touch a winch rope under tension.
- 03** Always wear appropriate PPE. Minimum requirement: safety boots, gloves, safety helmet, safety glasses and high visibility clothing.
- 04** Always check the winching layout and capacities of equipment ('walk the line').
- 05** Always obey rope-handling procedure.
- 06** Never wrap winch ropes around loads or attach rope hook back onto rope to form loop.
- 07** Carry out regular inspections of winch rope.
- 08** Always locate position of emergency stop before attempting to use winch.
- 09** Always maintain 5 wraps of rope around winch drum.
- 10** Never operate winch drum free spool with winch rope attached to load.
- 11** Always survey site for potential hazards before deploying winching equipment.
- 12** Always survey path load will be winched through for potential hazards.
- 13** Do not use winch for movement of persons.

## ▶ TRANSPORTER WINCH OPERATION

- As with any powerful tool there are hazards involved with winching operations. However with proper training and risk awareness coupled with well-maintained equipment and respect for the power available, the operator should come to regard winching equipment as a most versatile and efficient tool.

- The winch may be controlled by either a wander lead/spool valve control or by a radio remote handset, which gives the operator freedom of movement without the encumbrance of trailing wires.

NOTE: The winch will only 'winch out' if there is a load on the winch rope. To avoid a badly spooled winch drum and rope damage do not 'winch out' without tension on the rope.

- A free spool control is provided on the end of the winch. This control disconnects all braking and drive from the drum, which is free to rotate. It is used when a length of S.W.R. needs to be unwound by hand.

Under no circumstances attempt to engage free spool with a load attached to the winch rope.

- At all times it should be remembered that winching operations involve tensile forces, which are difficult to restrain in the event of a failure within the winching system. In the event of a failure the tensile forces will suddenly be released causing the winching equipment and winch rope to 'whiplash', as a piece of released elastic. Bodily contact with a winch rope suddenly released in this manner may cause serious injury. In addition the load will be released and may move uncontrollably possibly causing further injury.
- Before using a winch, identify the location of the emergency stop/isolator switch.
- To prevent discharge of vehicle battery, ensure engine is running when using an electric winch.
- The maximum power available from a winch is when the rope is on the bottom layers of the drum. Therefore avoid 'overloading' the drum with excessive length of rope.
- When handling the winch rope always wear good rigger gloves.
- To maintain the winch rope in a safe condition good rope management is essential – keep the rope as square to the drum as possible – maintain tension on rope by hand when powering 'out' under no load – maintain tension on rope by hand when powering 'in' under no load periodically pull all the rope off the drum and rewind under hand tension to achieve neat and tightly packed wraps of rope on the drum.
- Never allow the winch rope to slide through the hands. Always handle winch rope 'hand over hand'. Always ensure gloves are worn when handling steel wire rope.

- Inspect the rope and end fittings daily for damage and deformation. Check regularly for signs of wear in the form of broken strands or severe kinks along its length. If there are more than 6 strands broken in any length of 100mm the rope is severely weakened and must be replaced.
- Safety catches must be in place on hooks.
- Keep yourself and all personnel clear of the winch rope.
- Never step over, stand near or touch a winch rope under tension.
- Never wrap the winch rope around a load or hook the rope back in itself.
- Never pass the winch rope around any object other than a snatch block or sheave specially designed for the purpose.
- Always maintain 5 wraps of rope on the drum – the rope anchor on the drum will not withstand the winch pull without these 5 wraps on the drum. This is of particular importance when winching 'out'.
- Always ensure that the drive is fully engaged to the winch and that the free spool is not partially selected before attempting to winch a load.
- Never operate the winch drum to free spool if the rope is attached to the load.
- Always survey the site for potential hazards before deploying the winching equipment.
- Ensure that any additional equipment used such as shackles, webbing straps etc are compatible to the winch capacity.
- Always survey the path the load will be winched along for potential hazards.
- Do not use the winch for the movement of persons.
- Always keep the 'down hill side' of the load clear of persons.
- Do not use the winch rope as a towrope or pull the rope of the drum by engaging free spool, attaching the rope to the load and driving away.
- When repositioning the rope on the load always stabilise the load before removing winch rope.
- Ensure attachment point to load is secure and will forces.
- Position winch rope attachment to load centrally. The use of 'brothers' on two webbing straps is often required.
- Before applying tension to the load check winch rope is not fouling other components and all attachments are secure.

- Do not use the winch to secure the load on the body. After securing the load with cargo restrain equipment operate winch 'out' to just release the winch tension. When unloading first ensure tension is replaced in winch rope by momentarily operating winch 'in' before removing cargo restraints.
- The driver should not be positioned between the load and the winch or on the 'downhill' side of the load.
- When unloading do not allow the load to 'snatch' or operate the winch 'out' in a series of 'blips'. This will cause severe shock loading to the rope and winch resulting in possible failure.
- Before releasing the winch rope from the load ensure the load is stabilised and that there is no danger of the load 'running away'.
- Apply machine handbrake if fitted. Ensure machine is secure, loose items secured etc.
- 'Blip' winch 'out' control to just release tension on winch rope.
- Stow hydraulic loading ramps and ensure their security.
- Return air suspension to 'ride' condition.
- Turn battery isolator switch/emergency stop to 'off'.
- During journeys periodically check security of load restraints.

## ➤ UNLOADING TRANSPORTER BODY

- Ensure that the area is segregated, an exclusion zone is in place and ground conditions are firm and stable.
- Dump air suspension.
- Deploy ramp to load position. Ensure ground end of the ramp tip is in contact with ground. If jacklegs are fitted they must **ALWAYS** be in firm contact with the ground. Block if necessary.
- Ensure there is sufficient space at base of ramps to unload machine.
- 'Blip' winch 'in' control to restore tension on the winch rope.
- Do not drive machine off transporter, use the winch. Some machines require being driven and winched.

- Take great care not to shock load the winch. When machine passes over change of angle between beavertail and flat part of body the machine may roll rearwards. Carry out this part of the operation as slowly as possible ensuring that the winch rope does not go slack.
- **THERE ARE ONLY TWO PLACES A DRIVER SHOULD BE POSITIONED:**
  - Alongside the body on the ground.
  - In the normal driving position of the machine being unloaded if it requires to be driven and winched at the same time.
- Keep 'downhill' side of operation clear of all personnel.
- Release handbrake.
- Ensure machine is in neutral gear. Release steering lock if fitted.
- Release load restraints.
- Commence winch 'out' operation, do not operate winch in a series of 'blips', and load in one continuous operating. Ensure that machine rolls off body controlled by winch rope.
- **IF MACHINE STOPS, IMMEDIATELY STOP WINCH OPERATION.**
- When machine is clear of ramps, secure machine, apply handbrake and remove winch rope.
- Stow winch rope and load restraints. Stow loading ramps.
- Return air suspension to 'ride' condition.

## ➤ LOADING THE TRANSPORTER BODY

- Ensure that the area is segregated, an exclusion zone is in place and ground conditions are firm and stable.
- Turn battery isolator switch/emergency stop to 'on'.
- Dump air suspension if vehicle fitted with air suspension.
- Deploy ramp to load position. (See operation of Hydraulic Loading Ramps).
- Ensure ground end of the ramp tip is in contact with ground.

- If jacklegs are fitted they must always be in firm contact with ground. Block if necessary.
- Position machine to be loaded centrally at base of ramps with wheels touching ramps.
- Do not drive machine onto transporter, use the winch.
- Ensure machine to be loaded has handbrake 'on' or if not fitted with handbrake will not roll away whilst preparing for loading, check if necessary.
- Deploy winch rope (See the Transporter Winch section).
- Select attachment point on machine and fit winch rope hook - it may often be necessary to use a strop or shackle onto the attachment point before fitting hook. Ensure attachment point is secure. If in doubt use brothers for central pull from secure parts of the machine.
- Operate bypass valve on hydraulic: driven machines if fitted.
- Ensure vehicle is in neutral gear. Release steering lock if fitted.
- Release machine handbrake.
- Commence winch operation: do not operate winch in a series of 'blips' and load in one continuous operation.
- Keep 'downhill' side of operation clear of all personnel.
- Operators shall not position themselves between machine and transporter body headboard.
- During loading it may be required to steer the machine, this may be done by pushing the front wheels of the machine from the ground whilst the operator stands on the ground alongside the body.

**DO NOT WALK UP THE BODY ALONGSIDE THE MACHINE BEING LOADED WITH A HAND ON THE STEERING WHEEL.**

**THERE ARE ONLY TWO PLACES A DRIVER SHOULD BE POSITIONED:**

- Alongside the body on the ground.
- In the normal driving position of the machine being loaded if it requires to be driven and winched at the same time.
- Once machine is positioned on transporter body fit load restraints to each wheel or lashing points on machine. First restraint fitted should restrain machine from roll 'off'. Load restraints should restrain the machine in all directions.

## ➤ **GUIDANCE TO THE SAFE USAGE OF PLANT TRANSPORTER TYPE BODY FITTED WITH ELECTRIC WINCH AND HYDRAULIC LOADING RAMP**

### **THE FOLLOWING IS FOR GUIDANCE ONLY**

- Each user must carry out a dynamic risk assessment for the type of operation the equipment is used for and act to minimise the risks identified.
- When operating at the roadside the danger from other road users place operators at high risk.
- Before attempting to load/unload the body, ensure the area to work in is safe and secure. This may involve deploying warning beacons (light bars), cones, or requesting help from the site supervisor. Ensure your actions do not cause a hazard to other road users.

#### **POWER ISOLATION**

- A power isolation switch is fitted on the vehicle. Ensure this is off when the winch or ramp is not in use.
- Both the winch and the ramp draw power from the vehicle battery. When operating either, ensure engine is running to prevent battery discharging.

#### **EMERGENCY STOP**

- Before operation locate position of Emergency Stop on vehicle. In an emergency depress stop button, to reset twist button.

#### **OPERATION OF HYDRAULIC LOADING RAMP**

- The control station for the ramp is situated at the nearside of the vehicle. Stays are fitted to restrain ramp during travel.



## ➤ RAMP OPERATION

### STANDARD & FLIP-OVER RAMPS

#### Lowering Ramps

- Lower jacklegs.
- 'Dump' air from air suspension.
- NOTE: Never lower ramps before dumping suspension, always dump suspension first.
- When ramp is in use or being deployed, keep area behind ramp clear of all personnel.
- Release the stays at each side of the ramp.

**WHEN RELEASING STAYS DO NOT STAND BEHIND THE RAMP, STAND TO THE SIDE CLEAR OF THE RAMP. RELEASE OFFSIDE STAY FIRST.**

- Ensure area behind ramp is clear; exclusion zone to be adhered to at all times.
- Lower ramp. Continue to lower until ramp tip is firmly on the ground. If flip-over ramps are fitted then the feet on the end of the upper section must also be firmly on the ground.
- The ramp is now ready for loading.

## ➤ RAMP OPERATION continued

#### Raising Ramp

- When ramp is in use or being deployed keep area behind ramp clear of all personnel.
- Ensure no part of the load will obstruct the ramp when raised to the stowed position.
- Ensure all personnel are kept clear when stowing ramp.
- Raise ramp. Continue to raise until ramp is firmly against stops.
- Keep all personnel and limbs away from moving ramp.
- Engage stay on nearside first.

**WHEN ENGAGING STAYS DO NOT STAND BEHIND RAMP, STAND TO THE SIDE CLEAR OF THE RAMP. ENGAGE NEAR SIDE STAY FIRST.**

- Under no circumstances attempt to drive without stays being engaged.
- Return air suspensions to 'ride' condition.
- Raise jacklegs.

#### SLIDING RAMPS

Ensure ramps are fully vertical.

- Remove stays.
- Slide ramps.
- Never use slider when ramps are lowered. Always slide ramps out level with side of body and fit stays before travelling.

#### USING CHEESEWEDGE RAMPS

On vehicles fitted with folding cheesewedge ramps, operate as follows:

- Dump air suspension and ensure jacklegs are in firm contact with the ground.
- When in straightened position, the ramps are to be operated with the same method as standard ramps. **BUT** operator **MUST** ensure the locking pins are fully engaged on both sides of ramp before use with a visual check.
- **Fully open and close the ramps at least once per day.**



**To fold ramps**

Ensure straightened ramps are vertical and fully against stops.

- Operate folding control button or spool.
- Locking pins will automatically retract and ramp will fold.
- To reverse operation, operate opposite control, ramps will straighten, and locking pin will engage.

**ALWAYS: Visually check the locking pins have fully engaged before use.**

**HYDRAULIC BEAVERTAIL**

**To raise ramps:**

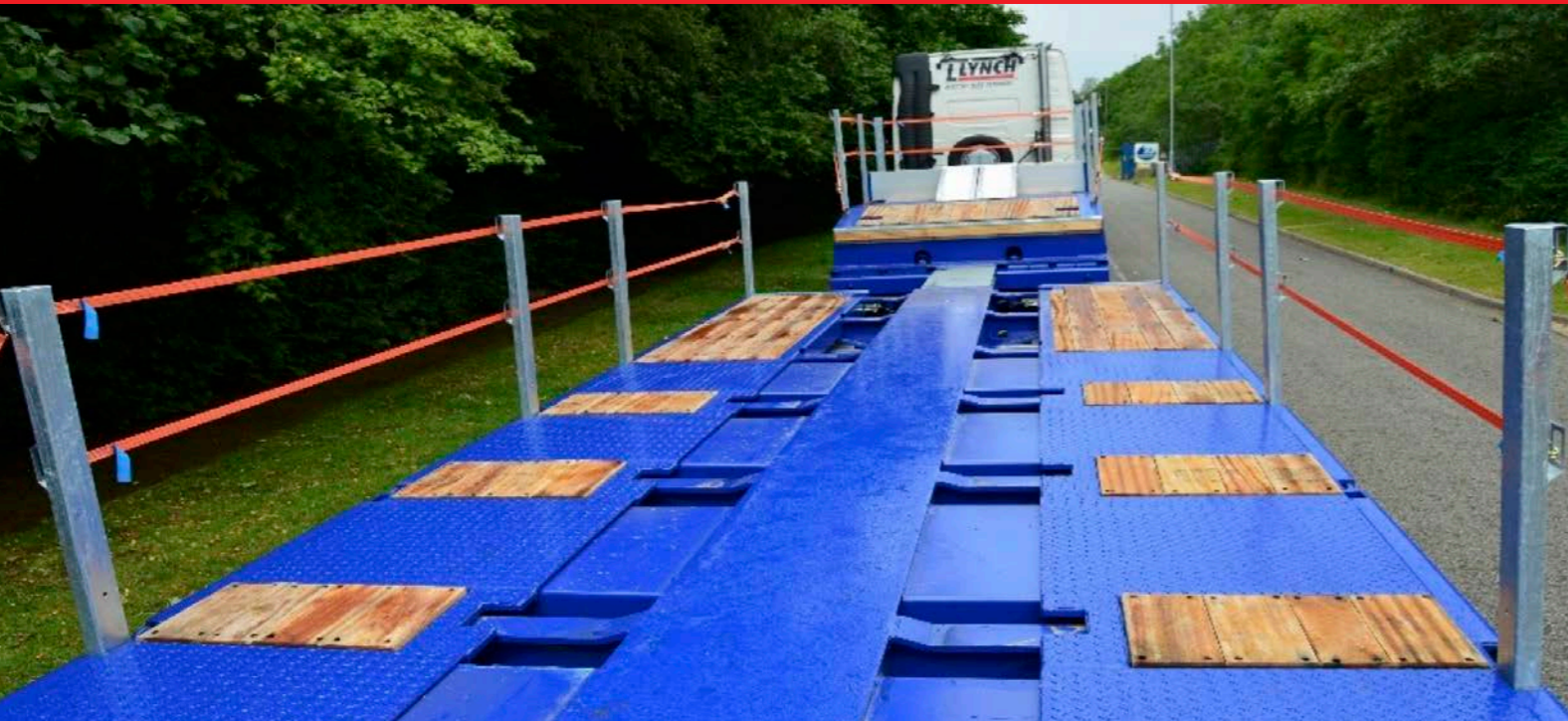
- Ensure ramps are fully vertical.
- Raise tail to full height.
- Engage travel stays.

**To lower ramps:**

- Release travel stays.
- Lower tail onto chassis.

**EXTENDING THE LOAD BED**

- This can be carried out either before or after the ramps are deployed; but beware **DRIVERS MUST RELEASE THE RAMP RESTRAINTS** before carrying out the operation.
- **DRIVERS MUST** also release the side protection / fall arrest straps between the load bed and the neck.
- Once the above is complete, energise one side at a time and operate the control lever to extend the hydraulic deck to the desired width.
- Complete the operation by operating the opposite side of the trailer to the desired width.



**FALL ARREST BETWEEN NECK AND LOAD BED (SEE PHOTO ABOVE LEFT)**

- Drivers will have removed two straps, one extending from the top of the front load bed post to the top of the first neck post. The second extending from the top of the front load bed post to the bed anchor point on the slope of the neck.

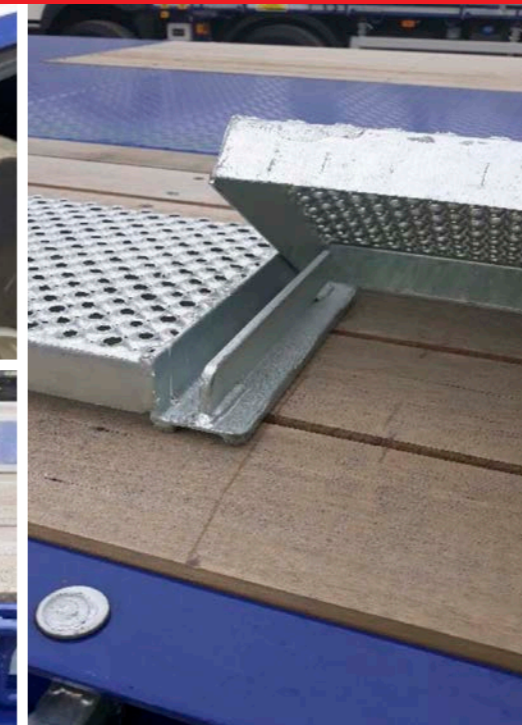
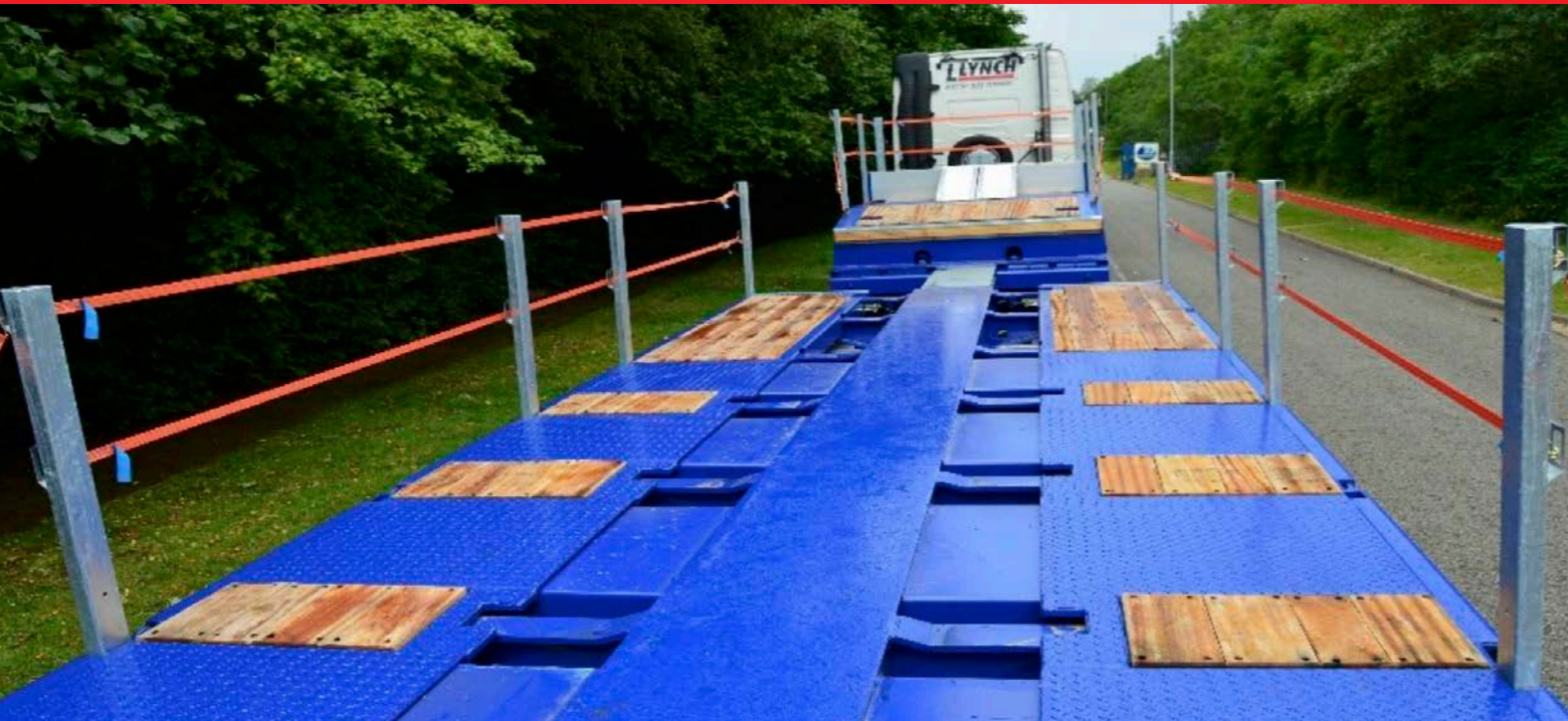
**BED SET TO THE DESIRED WIDTH**

- Note the sizeable gaps in the bed created by the adjustment of the load bed.
- Note how, with the simple removal of four straps, the remainder can stay in place.



**STRETCHING THE TRAILER LOAD BED**

- The operation of stretching the bed is as follows but please note...  
**DRIVERS MUST NEVER CARRY OUT THIS OPERATION WITH THE LEGS DOWN**
- Once the unit and trailer are positioned correctly and with the fall arrest removed between the load bed and the neck of the trailer, drivers should apply the trailer park brake.
- Activate the trailer extension release locking pin pull the lever up.
- Drivers will see a yellow indication bar protrude from the N/S of the trailer when the locking pin has been released.
- Get back into the cab of the unit start the engine, build air if necessary and level the unit and trailer combination using the wander lead of the unit.
- The driver can now draw forward the desired distance to lengthen the load bed of the trailer.
- Once at the desired length stop, drivers should apply park brake of the unit and dismount the vehicle.



- Activate the trailer release locking pin pull the lever down.
- The yellow indication bar will slide back into the trailer if perfectly aligned (however this is rarely the case) if not drivers should:
- Get back into the unit and move it slowly forward or rearward in order to engage the locking pin.
- The operation is now complete, the driver can disengage the trailer brake.

**Closing the trailer is the same exactly process reversed.**

## ➤ BEAVERTAIL FALL ARREST AND CATWALK

- Turn out bars are stowed against the side of the loadbed when running. They sit behind lugs and are easily lifted up and turned out.
- The bars are held in place by the tension of the straps only.
- There are three walkways, which all have a male and female end.
- They fit together as in the picture.
- These should be deployed on the side of the load bed which you will be dismounting the vehicle / plant which has been loaded.

**TAKE CARE WHEN HANDLING THESE!  
THEY ARE FRESHLY GALVANIZED. WEAR GLOVES AT ALL TIMES.**

- Once the fall arrest is deployed it will look like the photo.
- Ensure that the yellow straps are in place and that they are tensioned in order to keep the fall arrest assembly in place.
- Catwalk sections are stowed safely in a rack accessed under the ramp controls on the N/S of the vehicle.
- The fall arrest must be fitted when practicable and /or when site rules specify. When carrying a 14 ton excavator it will be necessary to collapse and stow the fall arrest system.

## ➤ WORK POSITION BELT & LANYARD

### INSPECTION AND PRE-USE CHECKS

- The belt and lanyard should be used only for the purpose intended.
- A thorough visual inspection should be carried out before use; this inspection should include (but is not limited to) the following:
  - Check all webbing and lanyard for signs of cuts, abrasions, fraying, tears, burns, mold, discolouration, or chemical attack (PUWER 1998).
  - Check all stitching for signs of loose threads, pulling or cuts. Stitch patterns must be identical on both sides.
  - Buckles and D rings must be free from rust, discoloration, excessive wear or cracks.
  - If the belt or lanyard become dirty do not use cleaning detergents as this may degrade the work positioning equipment.
  - If the belt or lanyard becomes damaged in any way **DO NOT** use.
  - All belts and lanyards will be checked annually; and have a maximum service period from point of issue of five years.



The belt is fastened using a male and female coupler.

The male end of the belt is passed through the female.

Until the male end is through and seated on top of the female coupler. A double tug should be performed to confirm that the belt is securely fastened.



The lanyard is secured to the belt by means of a screw gate karabiner. Ensure that this is tightened securely.

The lanyard is attached to anchorage points by means of a gated hook.

One handed operation for opening and closing the gate.

## ➤ SELECTING AND ATTACHING TO ANCHORAGE POINTS



Care should be taken when selecting anchorage points.

Ensure that the gate is fully closed; and that a double tug has been performed before going ahead.

➤ ACCESS AND EGRESS FROM LOAD BEDS



Always maintain three points of contact; and use correct points of access and egress.

If there is no other option then ramps may be used to gain access to the load bed; Three points of contact must always be maintained, ensure that the load bed is clear of obstructions and debris. If the ramps are icy then lay down salt or sand to improve grip.

If it is not practicable for the fall arrest system to be used, then a work positioning belt and safety lanyard must always be used when getting on or off of items of plant on the load bed.

➤ ACCESS AND EGRESS FROM PLANT ITEMS ON THE LOAD BED



Drivers will remain fully seated with the seatbelt engaged until they have clipped onto an anchorage point ensuring that this is secure using a double tug and checking the gate is closed.



Maintain three points of contact when accessing and egressing the machine; Never face forward or jump down.

Do not remove the positioning lanyard.



Drivers standing with both feet on the load bed may remove the positioning lanyard but only if the fall arrest is up. If the fall arrest is not up, then continue using the positioning lanyard.



The catwalk should be deployed to the side of the vehicle where drivers will access and egress the item of plant.

➤ ACCESS AND EGRESS FROM LOAD BEDS



Low loaders or semi-trailers can be accessed from the side as long as three points of contact are maintained throughout.



If there is no other option, then ramps may be used to gain access to the load bed; Walk in the middle of the ramp **DO NOT RUN**. Ensure that the load bed is clear of obstructions and debris. If the ramps are icy then lay down salt or sand to improve grip.



Machine cabs will often be above the fall arrest systems on the trailers; therefore, work positioning lanyards should be used when accessing and egressing cabs.



Maintain three points of contact when accessing and egressing the machine; Never face forward or jump down. Do not remove the positioning lanyard.



Once a driver is standing with both feet on the load bed they may remove the positioning lanyard but only if the fall arrest is up. If the fall arrest is not up, then continue using the positioning lanyard.

**ENSURE THAT ALL EQUIPMENT IS SERVICEABLE AND FIT FOR PURPOSE BEFORE USE.**

**ENSURE THAT YOU ARE FAMILIAR WITH AND HAVE HAD THE CORRECT TRAINING BEFORE ATTEMPTING ANY OPERATION.**

**IF THE SITUATION CHANGES STOP!**

# SSOW: Loading/Unloading Plant Transporters

Issue Number: 05    Date of Issue: 11/1/23    Based on: RA21

## PPE TO BE WORN

- High-vis Vest / Jacket
- Safety Boots
- Safety Gloves (As req'd)
- Safety Helmet (As req'd)



For this activity, the following PPE must be worn

## SAFE SYSTEM OF WORK

- Load bed and ramps to be kept free of debris, mud, oil, grease, waste, ice etc. Sand to be spread where necessary to aid grip. Tracks/Wheels of plant to be cleaned, if necessary, before loading.
- Lashing chains, slings, timber, ropes etc. to be stowed safely when not in use so as not to create trip or falling hazards.
- Maintain 3 points of contact, if possible, when working near the edge of the load bed.
- Only competent, authorised personnel to load/unload.
- Staff to follow safe loading procedures for each machine type in the Transport Safety Manual when loading/unloading.
- Plant to be secured to Transporters as per manufacturer's procedures and Transport Safety Manual procedures and guidance.
- Keep unauthorised personnel away from transporters. Use banksmen where possible to control pedestrian movement.
- Plant to be guided by a competent banksman standing in a safe place during loading/unloading.
- Drivers to receive familiarisation training for each item of plant being transported. Training records to be maintained.
- Ensure the transporter is parked on firm and level ground, if this is not possible, and on incline ensure that the vehicle is facing uphill.
- Anti - Roll bars fitted to dumpers, rollers and mini excavators etc. are to be locked in the raised position at all times during loading/unloading.
- unloading. Seat belts are to be worn on all machines at all times.
- To prevent drivers/operators being crushed in the event of a plant machine rolling over - Never try to jump clear - stay in the seat with the seat belt fastened
- Do not jump off the transporter load bed. Walk down the ramps or sit down and lower yourself gently off the edge.
- Never work directly underneath overhead power cables.
- Never work within 9 metres horizontally of overhead power cables supported on wooden poles.
- Never work within 15 metres horizontally of overhead power cables supported on steel pylons or structures.
- Always follow safe systems of work and routes provided on construction sites when driving under overhead power cables.
- Keep hands clear of pinch points and crush zones on ramps and trailers.
- All loading/unloading to be carried out slowly and safely - do not rush.
- When collecting plant from sites ensure plant is free from defects which could affect safe loading. If in doubt stop work, do not move plant and seek advice from the transport office
- Always liaise with our clients site staff to ensure on site risks are understood and controlled

## IDENTIFIED RISKS

The SSOW above has been compiled after identifying the following risks from this activity:

- Falls of people or objects from the transporter load bed and ramps
- Falls of vehicles from the transporter load bed and ramps
- Slips & Trips
- Personnel being struck by moving or falling vehicles
- Electrocution from contact with overhead power cables
- Personal injuries

## LOADING/UNLOADING PROCEDURES

- Identify area for loading/ unloading. Ensure the vehicle is parked on firm and level ground, if this is not possible, and on incline ensure that the vehicle is facing downhill, auxiliary brake applied where required. Loading/unloading must not be carried out in proximity to excavations and overhead obstructions.
- Keep clear of pedestrian and vehicle routes wherever possible. If this is not possible ensure the area is appropriately segregated.
- Set load bed for operation to take place; if the load projects 1/3 of the tracks or wheels deploy outriggers or widen bed.
- Deploy vehicle/trailer jacklegs during all plant loading / unloading activities.
- If the load projects 305mm or width of 2.9m (but not more than 3.5m) fit red white markings (if at night fit lights).
- Identify machine and configure it for operation.
- If using a banksman confirm lines of communication.
- Ensure only those involved in the operation are in the working area. Establish an exclusion zone where possible.
- Plant operator to leave plant/machine outside exclusion zone and hand over to vehicle driver.
- Only the HGV / plant delivery driver with A50/68 competencies are permitted to load plant/machine onto vehicles/trailers.
- Confirm (visual check) that the machine is as central to the load bed ramps as possible (equidistant).
- Select the correct gear / engine speed for the loading/unloading operation.
- Commence loading being as steady and fluid as possible; DO NOT RUSH.
- Position the item of plant on the load bed, in optimum place for safe restraint and transit.
- If using the low loader neck, ensure that the supplied ramps are used to load the neck of the vehicle. Where loading attachments onto the neck, this is to be placed at the optimum load point and secured accordingly.
- Plant loaded as low as possible, as narrow as possible as short as possible.
- Secure plant to load bed or trailer as per manufacturers guidelines and L Lynch Transport Safety Manual.
- Secure attachments as required.
- Make transporter ready for transit. Also ensure that you stow all loose items prior to departure.
- Measure highest point of transporter/ load using measuring stick.
- Change in cab height indicator.
- After a short time in transit stop and check load restraints.
- If the load situation or condition changes STOP and check load / call Transport Office.
- If unsure, STOP call Transport Office.
- Always make sure you have had the correct Information, instruction and training prior to commencement of any plant operation or transportation.

# SSOW 05b: Loading/Unloading Solid Drum Roller - Transporters

Issue Number: 01

Date of Issue: 18/01/23

Based on: RA21

## PPE TO BE WORN

- High-vis Vest / Jacket
- Safety Boots
- Safety Gloves
- Safety Helmet
- Safety Glasses
- (Site/Contract Specific)



For this activity, the following PPE must be worn

## SAFE SYSTEM OF WORK

- All Lynch transport drivers must report to site security/office on arrival at any customer site and wait further instruction before continuing (Site Escort may be required).
- Once the driver has been directed to the plant loading/unloading area contact must be made with site Black/Grey Hat and confirm if a Point of Works Risk Assessment and Task Briefing is required prior to any loading/unloading activity can commencing.
- Always communicate with site staff to ensure all on-site risks have been identified, controlled and you have understood any information received.
- Only qualified (CPCS A49/A50/68 NPORS plant mover), competent and authorised personnel to load/unload plant on Lynch transport vehicles.
- All Lynch drivers are trained in working at height and refer to RA20 if required.
- Staff to follow safe loading procedures for each machine type in the Transport Safety Manual when loading/unloading.
- Drivers must ensure they have received familiarisation training for the item of plant being loaded/unloaded. (Training records to be maintained).
- Lynch drivers must follow safe loading procedures for each machine type as documented in the Transport Safety Manual when loading/unloading plant.
- Ensure the transporter vehicle is parked on firm and level ground prior to any loading/unloading activity being carried out. (NEVER position the transporter vehicle facing up hill as this will increase the load bed entry angle).
- The driver will Maintain 3 points of contact when entering/exiting the vehicle cab at all times and when working near the edge of the load bed (when possible).
- The vehicle load bed and ramps will be kept free of debris, mud, oil, grease, waste, ice and other possible slip/trip hazards. (Sand to be spread where necessary to increase traction).
- Ensure vehicle/trailer jackleg/ramps are in place prior to all plant loading / unloading activities.
- Keep all limbs clear of pinch points and crush zones on ramps and trailers when setting load bed to receive Roller.
- In adverse weather conditions use sand/grit to increase roller drum traction.
- When loading Rollers, the solid drum of roller is to be cleaned if necessary, (before loading).
- Three points of contact to be maintained including the use of factory fitted handrails/holds and foot holds provided.
- Always follow the sites safe systems of work and routes provided when driving under overhead power cables.
- DO NOT work directly underneath overhead power cables.

## SAFE SYSTEM OF WORK

- Do NOT work within 9 metres horizontally of overhead power cables supported on wooden poles.
- DO NOT work within 15 metres horizontally of overhead power cables supported on steel pylons or structures.
- When collecting Rollers from sites ensure plant is free from defects which could affect the safe loading, carry out a rolling brake check in a controlled manner prior to loading. (If in doubt stop work, do not move Roller and seek advice from the transport office).
- Rollers loading/unloading to be guided by a competent site banksman (where possible) standing in a safe place during loading/unloading.
- Ensure all unauthorised personnel are kept a safe distance from plant/transporter loading/unloading. Use banksmen where possible to control pedestrian/plant interface.
- Banksman to remain a safe distance (outside red zone) during loading/unloading activities. (If the driver loses sight of the banksman the driver must STOP until contact has been re-established).
- To prevent drivers/operators being crushed in the event of a plant machine rolling over ensure seatbelts are worn at all times and ROPS are in place (if provided) never try to jump clear – stay in the seat with the seat belt fastened.
- All loading/unloading to be carried out safe, controlled manner – Do Not Rush.
- Rollers to be secured to transporters as per manufacturer's guidance and Transport Safety Manual procedures.
- Never jump off the transporter load bed, walk down the ramps or sit down on load bed and lower yourself gently off the edge.
- Lashing chains, slings, timber, ropes etc. to be stowed safely when not in use so as not to create trip or falling hazards.
- After Roller is loaded and secured, jacklegs and ramps to be stowed (driver to keep all limbs clear of pinch points and crush zones on ramps and trailers).
- If any part of the loading/unloading of plant falls outside of the above at any time or the operation does not go as planned STOP IMMEDIATELY and report to site Black/Grey Hat and Lynch Transport Office DO NOT CONTINUE until a recovery solution has been agreed by Lynch Transport Office and customer.

## IDENTIFIED RISKS

The SSOW above has been compiled after identifying the following risks from this activity:

- Falls from height, people or objects from the transporter load bed and ramps
- Falls from height of vehicles drivers from the transporter load bed and ramps
- Slips & Trips (injuries sustained, sprains, abrasions, lacerations).
- Driver injuries from moving vehicle/trailer parts (ramps/jacklegs).
- Site personnel struck by moving vehicles and plant. (PPI).
- Electrocution from contact with overhead power cables.

# SSOW 05b: Loading/Unloading Solid Drum Roller - Transporters

Issue Number: 01 Date of Issue: 18/01/23 Based on: RA21

## LOADING/UNLOADING PROCEDURES

- On arrival to site, driver must follow site procedures/rules/traffic management routes.
- Identify area for loading/ unloading. Ensure the vehicle is parked on firm and level ground, DO NOT position the transporter facing up hill as this will increase the load bed entry angle
- Driver to receive POWRA/Task Briefing if required.
- Engage trailer brake as required to restrict trailer movement and increase trailer stability.
- Loading/ unloading must not be carried out in proximity to excavations and overhead obstructions.
- Secure vehicle/trailer jacklegs for all plant loading / unloading activities.
- Set load bed for operation to take place; if the load projects 1/3 of the roller drum set trailer to correct width as required by means of factory fitted greedy boards or widen trailer bed.
- If the load projects 305mm or width of 2.9m (but not more than 3.5m) fit red white markings (if at night fit lights).
- If a banksman is required, confirm lines of communication and ensure that the banksman stands in a safe position (outside the Red Zone) during all plant loading/unloading operations, if you lose sight of the banksman STOP until contact has been re-established.
- Establish an exclusion zone prior to all operations if site conditions/set up permits.
- Plant/Roller operator MUST hand over plant/ Roller control to the vehicle driver outside exclusion zone (plant operators are not permitted to load any plant onto a vehicle load bed).
- Only the HGV / plant delivery driver with CPCS A49/A50/68 or NPORS plant mover competencies are permitted to load any plant onto vehicles/trailers.
- The driver will Select the correct gear / engine speed for the loading/unloading operation.
- All loading/unloading to be carried out safe, controlled manner - Do Not Rush.
- Confirm (visual check) that the machine is as central to the load bed ramps as possible (in the middle) and secure to load bed as per manufacturer's guidance and Transport Safety Manual procedures.
- Ensure all doors and windows are closed (if fitted) and loose items secured in the cab before transit
- After Roller is loaded and secured, jacklegs and ramps to be stowed (driver to keep all limbs clear of pinch points and crush zones on ramps and trailers).
- Only exit customer sites when load is secure and agreed by site banksman or other representative (check if escort is required).
- Rollers are NOT permitted to be transported on trailer necks.
- If any part of the loading/unloading of plant falls outside of the above at any time or the operation does not go as planned STOP IMMEDIATELY and report to site foreman and Lynch Transport Office DO NOT CONTINUE until a recovery solution has been agreed by Lynch Transport Office and customer.

**FOR MORE INFORMATION, PLEASE REFER TO RISK ASSESSMENT 21**

# SSOW 05c: Loading/Unloading Tracked Excavators - Transporters

Issue Number: 01 Date of Issue: 18/01/23 Based on: RA21

## PPE TO BE WORN

For this activity, the following PPE must be worn

- High-vis Vest / Jacket
- Safety Boots
- Safety Gloves
- Safety Helmet
- Safety Glasses
- (Site/Contract Specific)



## SAFE SYSTEM OF WORK

- All Lynch transport drivers must report to site security/office on arrival at any customer site and wait further instruction before continuing (Site Escort may be required).
- Once the driver has been directed to the plant loading/unloading area contact must be made with site Black/Grey Hat and confirm if a Point of Works Risk Assessment and Task Briefing is required prior to any loading/unloading activity can commencing.
- Always communicate with site staff to ensure all on-site risks have been identified, controlled and you have understood any information received.
- Only qualified (CPCS A49/A50/68 NPORS plant mover), competent and authorised personnel to load/unload plant on Lynch transport vehicles.
- All Lynch drivers are trained in working at height and refer to RA20 If required.
- Staff to follow safe loading procedures for each machine type in the Transport Safety Manual when loading/unloading.
- Drivers must ensure they have received familiarisation training for the Tracked excavator being loaded/unloaded. (Training records to be maintained).
- Lynch drivers must follow safe loading procedures for each machine type as documented in the Transport Safety Manual when loading/unloading plant.
- Ensure the transporter vehicle is parked on firm and level ground prior to any loading/unloading activity being carried out.
- The driver will Maintain 3 points of contact when entering/exiting the vehicle cab at all times and when working near the edge of the load bed (when possible).
- The vehicle load bed and ramps will be kept free of debris, mud, oil, grease, waste, ice and other possible slip/trip hazards. (Sand to be spread where necessary to increase traction).
- Ensure vehicle/trailer jackleg/ramps (including neck ramps if required) are in place prior to all plant loading / unloading activities.
- Keep all limbs clear of pinch points and crush zones on ramps and trailers when setting load bed to receive tractor and trailer combination.
- In adverse weather conditions use sand/grit to increase traction.
- During the loading process, keep the arm of the excavator as low as possible. Stability is increased when the arm is lower
- Prior to loading any Tracked excavator from site ensure that plant items are free from any defects which could affect the safe loading. If in doubt STOP, do not attempt to move the tracked excavator, inform the Black/Grey Hat and contact Lynch Transport Office for further instruction.
- Checks should be made to the tracked excavator before operation takes place to ensure track tension is ok and that both tracks are operational If in doubt STOP and contact Lynch Transport Office for further instruction.

## SAFE SYSTEM OF WORK

- Three points of contact to be maintained including the use of factory fitted handrails/holds and foot holds provided.
- Always follow the sites safe systems of work and routes provided when driving under overhead power cables.
- DO NOT work directly underneath overhead power cables.
- Do NOT work within 9 metres horizontally of overhead power cables supported on wooden poles.
- DO NOT work within 15 metres horizontally of overhead power cables supported on steel pylons or structures.
- Ensure all unauthorised personnel are kept a safe distance from plant/transporter loading/unloading. Use banksman where possible to control pedestrian/plant interface.
- Banksman to remain a safe distance (outside red zone) during loading/unloading activities. (If the driver loses sight of the banksman the driver must STOP until contact has been re-established).
- To reduce drivers/operators being injured in the event of a plant machine rolling over, seatbelts are worn at all times.
- All loading/unloading to be carried out safe, controlled manner – Do Not Rush.
- The tracked excavator must be secured to transporters as per manufacturer's guidance and Transport Safety Manual procedures.
- Never jump off the transporter load bed, walk down the ramps or sit down on load bed and lower yourself gently off the edge.
- Lashing chains, slings, timber, ropes etc. to be stowed safely when not in use so as not to create trip or falling hazards.
- After tracked excavator is loaded and secured, jacklegs and ramps to be stowed (driver to keep all limbs clear of pinch points and crush zones on ramps and trailers).
- If any part of the loading/unloading of plant falls outside of the above at any time or the operation does not go as planned STOP IMMEDIATELY and report to site Black/Grey Hat and Lynch Transport Office DO NOT CONTINUE until a recovery solution has been agreed by Lynch Transport Office and customer.

## IDENTIFIED RISKS

The SSOW above has been compiled after identifying the following risks from this activity:

- Falls from height, people or objects from the transporter load bed and ramps
- Falls from height of vehicles drivers from the transporter load bed and ramps
- Slips & Trips (injuries sustained, sprains, abrasions, lacerations).
- Driver injuries from moving vehicle/trailer parts (ramps/jacklegs).
- Site personnel struck by moving vehicles and plant. (PPI).
- Electrocutation from contact with overhead power cables.

## LOADING/UNLOADING PROCEDURES

- Secure vehicle/trailer jacklegs for all plant loading / unloading activities.
- Set load bed for operation to take place, if the load projects 1/3 of the tracked excavator set trailer to correct width as required by means of factory fitted greedy boards or widen trailer bed.
- If the load projects 305mm or width of 2.9m (but not more than 3.5m) fit red white markings (if at night fit lights).
- If a banksman is required, confirm lines of communication and ensure that the banksman stands in a safe position (outside the Red Zone) during all plant loading/unloading operations, if you lose sight of the banksman STOP until contact has been re-established.
- Establish an exclusion zone prior to all operations if site conditions/set up permits.
- Plant operator MUST hand over tracked excavator to the vehicle driver outside exclusion zone (plant operators are not permitted to load any plant onto a vehicle load bed).
- Only the HGV / plant delivery driver with CPCS A49/A50/68 or NPORS plant mover competencies are permitted to load any plant onto vehicles/trailers.
- The driver will Select the correct gear / engine speed for the loading/unloading operation.
- All loading/unloading to be carried out safe, controlled manner – Do Not Rush.
- Ensure that any attachments to be loaded are placed within easy reach of the transporter
- Confirm (visual check) that the machine is as central to the load bed ramps as possible (in the middle) and secure to load bed as per manufacturer's guidance and Transport Safety Manual procedures.
- Ensure all doors and windows are closed (if fitted) and loose items secured in the cab before transit
- After the tracked excavator is loaded and secured, jacklegs and ramps to be stowed (driver to keep all limbs clear of pinch points and crush zones on ramps and trailers).
- Only exit customer sites when load is secure and agreed by site banksman or other representative (check if escort is required).
- If any part of the loading/unloading of plant falls outside of the above at any time or the operation does not go as planned STOP IMMEDIATELY and report to site foreman and Lynch Transport Office DO NOT CONTINUE until a recovery solution has been agreed by Lynch Transport Office and customer.

**FOR MORE INFORMATION, PLEASE REFER TO RISK ASSESSMENT 21**

## PPE TO BE WORN

- High-vis Vest / Jacket
- Safety Boots
- Safety Gloves
- Safety Helmet
- Safety Glasses
- (Site/Contract Specific)



For this activity, the following PPE must be worn

## SAFE SYSTEM OF WORK

- All Lynch transport drivers must report to site security/office on arrival at any customer site and wait further instruction before continuing (Site Escort may be required).
- Once the driver has been directed to the plant loading/unloading area contact must be made with site Black/Grey Hat and confirm if a Point of Works Risk Assessment and Task Briefing is required prior to any loading/unloading activity can commencing.
- Always communicate with site staff to ensure all on-site risks have been identified, controlled and you have understood any information received.
- Only qualified (CPCS A49/A50/68 NPORS plant mover), competent and authorised personnel to load/unload plant on Lynch transport vehicles.
- All Lynch drivers are trained in working at height and refer to RA20 If required.
- Staff to follow safe loading procedures for each machine type in the Transport Safety Manual when loading/unloading.
- Drivers must ensure they have received familiarisation training for the item of plant being loaded/unloaded. (Training records to be maintained).
- Lynch drivers must follow safe loading procedures for each machine type as documented in the Transport Safety Manual when loading/unloading plant.
- Ensure the transporter vehicle is parked on firm and level ground prior to any loading/unloading activity being carried out.
- The driver will Maintain 3 points of contact when entering/exiting the vehicle cab at all times and when working near the edge of the load bed (when possible).
- The vehicle load bed and ramps will be kept free of debris, mud, oil, grease, waste, ice and other possible slip/trip hazards. (Sand to be spread where necessary to increase traction).
- Ensure vehicle/trailer jackleg/ramps (including neck ramps if required) are in place prior to all plant loading / unloading activities.
- Keep all limbs clear of pinch points and crush zones on ramps and trailers when setting load bed to receive tractor and trailer combination.
- In adverse weather conditions use sand/grit to increase traction.
- Prior to loading any tractor from site ensure both plant items are free from any defects which could affect the safe loading. If in doubt STOP, do not attempt to move the tractor, inform the Black/Grey Hat and contact Lynch Transport Office for further instruction.
- The driver must check tyre pressures on both tractor and trailer are correct, and a rolling brake check must be carried out in a controlled manner prior to any loading taking place. If in doubt STOP and contact Lynch Transport Office for further instruction.

## SAFE SYSTEM OF WORK

- If loading a tractor with an attachment. DO NOT operate the attachment by use of the spool valve controls inside tractor cab.
- If loading/unloading a dual braked tractor, ensure that the brake pedals are locked together by locking bar to eliminate the brakes engaging independently of each other.
- Three points of contact to be maintained including the use of factory fitted handrails/holds and foot holds provided.
- Always follow the sites safe systems of work and routes provided when driving under overhead power cables.
- DO NOT work directly underneath overhead power cables.
- Do NOT work within 9 metres horizontally of overhead power cables supported on wooden poles.
- DO NOT work within 15 metres horizontally of overhead power cables supported on steel pylons or structures.
- Ensure all unauthorised personnel are kept a safe distance from plant/transporter loading/unloading. Use banksman where possible to control pedestrian/plant interface.
- Banksman to remain a safe distance (outside red zone) during loading/unloading activities. (If the driver loses sight of the banksman the driver must STOP until contact has been re-established).
- To reduce drivers/operators being injured in the event of a plant machine rolling over, seatbelts are to be worn at all times.
- All loading/unloading to be carried out safe, controlled manner - Do Not Rush.
- Tractor must be secured to transporters as per manufacturer's guidance and Transport Safety Manual procedures.
- Never jump off the transporter load bed, walk down the ramps or sit down on load bed and lower yourself gently off the edge.
- Lashing chains, slings, timber, ropes etc. to be stowed safely when not in use so as not to create trip or falling hazards.
- After the tractor is loaded and secured, jacklegs and ramps to be stowed (driver to keep all limbs clear of pinch points and crush zones on ramps and trailers).
- If any part of the loading/unloading of plant falls outside of the above at any time or the operation does not go as planned STOP IMMEDIATELY and report to site Black/Grey Hat and Lynch Transport Office DO NOT CONTINUE until a recovery solution has been agreed by Lynch Transport Office and customer.

## IDENTIFIED RISKS

The SSOW above has been compiled after identifying the following risks from this activity:

- Falls from height, people or objects from the transporter load bed and ramps
- Falls from height of vehicles drivers from the transporter load bed and ramps
- Slips & Trips (injuries sustained, sprains, abrasions, lacerations).
- Driver injuries from moving vehicle/trailer parts (ramps/jacklegs).
- Site personnel struck by moving vehicles and plant. (PPI).
- Electrocution from contact with overhead power cables.

## SSOW 05d: Loading/Unloading Tractors - Transporters

Issue Number: 01

Date of Issue: 18/01/23

Based on: RA21

### LOADING/UNLOADING PROCEDURES

- Secure vehicle/trailer jacklegs for all plant loading / unloading activities.
- Set load bed for operation to take place, if the load projects 1/3 of the tractor set trailer to correct width as required by means of factory fitted greedy boards or widen trailer bed.
- If the load projects 305mm or width of 2.9m (but not more than 3.5m) fit red white markings (if at night fit lights).
- If a banksman is required, confirm lines of communication and ensure that the banksman stands in a safe position (outside the Red Zone) during all plant loading/unloading operations, if you lose sight of the banksman STOP until contact has been re-established.
- Establish an exclusion zone prior to all operations if site conditions/set up permits.
- Plant operator MUST hand over tractor/trailer control to the vehicle driver outside exclusion zone (plant operators are not permitted to load any plant onto a vehicle load bed).
- Only the HGV / plant delivery driver with CPCS A49/A50/68 or NPORS plant mover competencies are permitted to load any plant onto vehicles/trailers.
- The driver will Select the correct gear / engine speed for the loading/unloading operation.
- All loading/unloading to be carried out safe, controlled manner - Do Not Rush.
- Confirm (visual check) that the machine is as central to the load bed ramps as possible (in the middle) and secure to load bed as per manufacturer's guidance and Transport Safety Manual procedures.
- Ensure all doors and windows are closed (if fitted) and loose items secured in the cab before transit
- After the tractor is loaded and secured, jacklegs and ramps to be stowed (driver to keep all limbs clear of pinch points and crush zones on ramps and trailers).
- Only exit customer sites when load is secure and agreed by site banksman or other representative (check if escort is required).
- If any part of the loading/unloading of plant falls outside of the above at any time or the operation does not go as planned STOP IMMEDIATELY and report to site foreman and Lynch Transport Office DO NOT CONTINUE until a recovery solution has been agreed by Lynch Transport Office and customer.

**FOR MORE INFORMATION, PLEASE REFER TO RISK ASSESSMENT 21**

## SSOW 05e: Loading/Unloading Wheeled Plant - Transporters

Issue Number: 01

Date of Issue: 18/01/23

Based on: RA21

### PPE TO BE WORN

- High-vis Vest / Jacket
- Safety Boots
- Safety Gloves
- Safety Helmet
- Safety Glasses
- (Site/Contract Specific)



For this activity, the following PPE must be worn

### SAFE SYSTEM OF WORK

- All Lynch transport drivers must report to site security/office on arrival at any customer site and wait further instruction before continuing (Site Escort may be required).
- Once the driver has been directed to the plant loading/unloading area contact must be made with site Black/Grey Hat and confirm if a Point of Works Risk Assessment and Task Briefing is required prior to any loading/unloading activity can commencing.
- Always communicate with site staff to ensure all on-site risks have been identified, controlled and you have understood any information received.
- Only qualified (CPCS A49/A50/68 NPORS plant mover), competent and authorised personnel to load/unload plant on Lynch transport vehicles.
- All Lynch drivers are trained in working at height and refer to RA20 If required.
- Staff to follow safe loading procedures for each machine type in the Transport Safety Manual when loading/unloading.
- Drivers must ensure they have received familiarisation training for the item of wheeled plant being loaded/unloaded. (Training records to be maintained).
- Lynch drivers must follow safe loading procedures for each machine type as documented in the Transport Safety Manual when loading/unloading plant.
- Ensure the transporter vehicle is parked on firm and level ground prior to any loading/unloading activity being carried out.
- The driver will Maintain 3 points of contact when entering/exiting the vehicle cab at all times and when working near the edge of the load bed (when possible).
- The vehicle load bed and ramps will be kept free of debris, mud, oil, grease, waste, ice and other possible slip/trip hazards. (Sand to be spread where necessary to increase traction).
- Ensure vehicle/trailer jackleg/ramps (including neck ramps if required) are in place prior to all plant loading / unloading activities.
- Keep all limbs clear of pinch points and crush zones on ramps and trailers when setting load bed to receive wheeled plant item/s.
- In adverse weather conditions use sand/grit to increase traction.
- Prior to loading any wheeled item of plant from site ensure that plant items are free from any defects which could affect the safe loading. If in doubt STOP, do not attempt to move the item of plant, inform the Black/Grey Hat and contact Lynch Transport Office for further instruction.
- The driver must check tyre pressures on the item of plant that they are correct, and a rolling brake check must be carried out in a controlled manner prior to any loading taking place. If in doubt STOP and contact Lynch Transport Office for further instruction.

## SAFE SYSTEM OF WORK

- If loading/unloading a dual braked item of plant, ensure that the brake pedals are locked together by locking bar to eliminate the brakes engaging independently of each other.
- Three points of contact to be maintained including the use of factory fitted handrails/holds and foot holds provided.
- Always follow the sites safe systems of work and routes provided when driving under overhead power cables.
- DO NOT work directly underneath overhead power cables.
- Do NOT work within 9 metres horizontally of overhead power cables supported on wooden poles.
- DO NOT work within 15 metres horizontally of overhead power cables supported on steel pylons or structures.
- Ensure all unauthorised personnel are kept a safe distance from plant/transporter loading/unloading. Use banksmen where possible to control pedestrian/plant interface.
- Banksman to remain a safe distance (outside red zone) during loading/unloading activities. (If the driver loses sight of the banksman the driver must STOP until contact has been re-established).
- To reduce drivers/operators being injured in the event of a plant machine rolling over, seatbelts are worn at all times.
- All loading/unloading to be carried out safe, controlled manner – Do Not Rush.
- The item of wheeled plant must be secured to transporters as per manufacturer's guidance and Transport Safety Manual procedures.
- Never jump off the transporter load bed, walk down the ramps or sit down on load bed and lower yourself gently off the edge.
- Lashing chains, slings, timber, ropes etc. to be stowed safely when not in use so as not to create trip or falling hazards.
- After any wheeled plant is loaded and secured, jacklegs and ramps to be stowed (driver to keep all limbs clear of pinch points and crush zones on ramps and trailers).
- If any part of the loading/unloading of plant falls outside of the above at any time or the operation does not go as planned STOP IMMEDIATELY and report to site Black/Grey Hat and Lynch Transport Office DO NOT CONTINUE until a recovery solution has been agreed by Lynch Transport Office and customer.

## IDENTIFIED RISKS

The SSOW above has been compiled after identifying the following risks from this activity:

- Falls from height, people or objects from the transporter load bed and ramps
- Falls from height of vehicles drivers from the transporter load bed and ramps
- Slips & Trips (injuries sustained, sprains, abrasions, lacerations).
- Driver injuries from moving vehicle/trailer parts (ramps/jacklegs).
- Site personnel struck by moving vehicles and plant. (PPI).
- Electrocutation from contact with overhead power cables.

## LOADING/UNLOADING PROCEDURES

- Secure vehicle/trailer jacklegs for all plant loading / unloading activities.
- Set load bed for operation to take place; if the load projects 1/3 of the tractor/trailer combination set trailer to correct width as required by means of factory fitted greedy boards or widen trailer bed.
- If the load projects 305mm or width of 2.9m (but not more than 3.5m) fit red white markings (if at night fit lights).
- If a banksman is required, confirm lines of communication and ensure that the banksman stands in a safe position (outside the Red Zone) during all plant loading/unloading operations, if you lose sight of the banksman STOP until contact has been re-established.
- Establish an exclusion zone prior to all operations if site conditions/set up permits.
- Plant operator MUST hand over control of the item of plant to the vehicle driver outside exclusion zone (plant operators are not permitted to load any plant onto a vehicle load bed).
- Only the HGV / plant delivery driver with CPCS A49/A50/68 or NPORS plant mover competencies are permitted to load any plant onto vehicles/trailers.
- The driver will Select the correct gear / engine speed for the loading/unloading operation.
- All loading/unloading to be carried out safe, controlled manner – Do Not Rush.
- Confirm (visual check) that the machine is as central to the load bed ramps as possible (in the middle) and secure to load bed as per manufacturer's guidance and Transport Safety Manual procedures.
- Ensure all doors and windows are closed (if fitted) and loose items secured in the cab before transit
- After the item of wheeled plant is loaded and secured, jacklegs and ramps to be stowed (driver to keep all limbs clear of pinch points and crush zones on ramps and trailers).
- Only exit customer sites when load is secure and agreed by site banksman or other representative (check if escort is required).
- If any part of the loading/unloading of plant falls outside of the above at any time or the operation does not go as planned STOP IMMEDIATELY and report to site foreman and Lynch Transport Office DO NOT CONTINUE until a recovery solution has been agreed by Lynch Transport Office and customer.

**FOR MORE INFORMATION, PLEASE REFER TO RISK ASSESSMENT 21**

# SSOW 05f: Loading/Unloading Wheeled Excavators - Transporters

Issue Number: 01

Date of Issue: 18/01/23

Based on: RA21

## PPE TO BE WORN

- High-vis Vest / Jacket
- Safety Boots
- Safety Gloves
- Safety Helmet
- Safety Glasses
- (Site/Contract Specific)



For this activity, the following PPE must be worn

## SAFE SYSTEM OF WORK

- All Lynch transport drivers must report to site security/office on arrival at any customer site and wait further instruction before continuing (Site Escort may be required).
- Once the driver has been directed to the plant loading/unloading area contact must be made with site Black/Grey Hat and confirm if a Point of Works Risk Assessment and Task Briefing is required prior to any loading/unloading activity can commencing.
- Always communicate with site staff to ensure all on-site risks have been identified, controlled and you have understood any information received.
- Only qualified (CPCS A49/A50/68 NPORS plant mover), competent and authorised personnel to load/unload plant on Lynch transport vehicles.
- All Lynch drivers are trained in working at height and refer to RA20 If required.
- Staff to follow safe loading procedures for each machine type in the Transport Safety Manual when loading/unloading.
- Drivers must ensure they have received familiarisation training for the item of plant Wheeled excavator being loaded/unloaded. (Training records to be maintained).
- Lynch drivers must follow safe loading procedures for each machine type as documented in the Transport Safety Manual when loading/unloading plant.
- Ensure the transporter vehicle is parked on firm and level ground prior to any loading/unloading activity being carried out.
- The driver will Maintain 3 points of contact when entering/exiting the vehicle cab at all times and when working near the edge of the load bed (when possible).
- The vehicle load bed and ramps will be kept free of debris, mud, oil, grease, waste, ice and other possible slip/trip hazards. (Sand to be spread where necessary to increase traction).
- Ensure vehicle/trailer jackleg/ramps (including neck ramps if required) are in place prior to all plant loading / unloading activities.
- Keep all limbs clear of pinch points and crush zones on ramps and trailers when setting load bed to receive wheeled excavator.
- In adverse weather conditions use sand/grit to increase traction.
- During the loading process, keep the arm of the excavator as low as possible. Stability is increased when the arm is lower.
- Prior to loading any wheeled excavator from site ensure that plant items are free from any defects which could affect the safe loading. If in doubt STOP, do not attempt to move the wheeled excavator, inform the Black/Grey Hat and contact Lynch Transport Office for further instruction.

## SAFE SYSTEM OF WORK

- The driver must check tyre pressures on the wheeled excavator to ensure they are correct, and a rolling brake check must be carried out in a controlled manner prior to any loading taking place. If in doubt STOP and contact Lynch Transport Office for further instruction.
- If loading/unloading a dual braked wheeled excavator, ensure that the brake pedals are locked together by locking bar to eliminate the brakes engaging independently of each other.
- Three points of contact to be maintained including the use of factory fitted handrails/holds and foot holds provided.
- Always follow the sites safe systems of work and routes provided when driving under overhead power cables.
- DO NOT work directly underneath overhead power cables.
- Do NOT work within 9 metres horizontally of overhead power cables supported on wooden poles.
- DO NOT work within 15 metres horizontally of overhead power cables supported on steel pylons or structures.
- Ensure all unauthorised personnel are kept a safe distance from plant/transporter loading/unloading. Use banksmen where possible to control pedestrian/plant interface.
- Banksmen to remain a safe distance (outside red zone) during loading/unloading activities.
- (If the driver loses sight of the banksman the driver must STOP until contact has been re-established).
- To reduce drivers/operators being injured in the event of a plant machine rolling over, seatbelts are worn at all times.
- All loading/unloading to be carried out safe, controlled manner - Do Not Rush.
- The wheeled excavator must be secured to transporters as per manufacturer's guidance and Transport Safety Manual procedures.
- Never jump off the transporter load bed, walk down the ramps or sit down on load bed and lower yourself gently off the edge.
- Lashing chains, slings, timber, ropes etc. to be stowed safely when not in use so as not to create trip or falling hazards.
- After wheeled excavator is loaded and secured, jacklegs and ramps to be stowed (driver to keep all limbs clear of pinch points and crush zones on ramps and trailers).
- If any part of the loading/unloading of plant falls outside of the above at any time or the operation does not go as planned STOP IMMEDIATELY and report to site Black/Grey Hat and Lynch Transport Office DO NOT CONTINUE until a recovery solution has been agreed by Lynch Transport Office and customer.

## IDENTIFIED RISKS

The SSOW above has been compiled after identifying the following risks from this activity:

- Falls from height, people or objects from the transporter load bed and ramps
- Falls from height of vehicles drivers from the transporter load bed and ramps
- Slips & Trips (injuries sustained, sprains, abrasions, lacerations).
- Driver injuries from moving vehicle/trailer parts (ramps/jacklegs).
- Site personnel struck by moving vehicles and plant. (PPI).
- Electrocution from contact with overhead power cables.

# SSOW 05f: Loading/Unloading Wheeled Excavators - Transporters

Issue Number: 01

Date of Issue: 18/01/23

Based on: RA21

## LOADING/UNLOADING PROCEDURES

- Secure vehicle/trailer jacklegs for all plant loading / unloading activities.
- Set load bed for operation to take place; if the load projects 1/3 of the load bed, set trailer to correct width as required by means of factory fitted greedy boards or widen trailer bed.
- If the load projects 305mm or width of 2.9m (but not more than 3.5m) fit red white markings (if at night fit lights).
- If a banksman is required, confirm lines of communication and ensure that the banksman stands in a safe position (outside the Red Zone) during all plant loading/unloading operations, if you lose sight of the banksman STOP until contact has been re-established.
- Establish an exclusion zone prior to all operations if site conditions/set up permits.
- Plant operator MUST hand over control of the item of plant to the vehicle driver outside exclusion zone (plant operators are not permitted to load any plant onto a vehicle load bed).
- Only the HGV / plant delivery driver with CPCS A49/A50/68 or NPORS plant mover competencies are permitted to load any plant onto vehicles/trailers.
- The driver will Select the correct gear / engine speed for the loading/unloading operation.
- All loading/unloading to be carried out safe, controlled manner - Do Not Rush.
- Confirm (visual check) that the machine is as central to the load bed ramps as possible (in the middle) and secure to load bed as per manufacturer's guidance and Transport Safety Manual procedures.
- Ensure all doors and windows are closed (if fitted) and loose items secured in the cab before transit
- After the item of plant are loaded and secured, jacklegs and ramps to be stowed (driver to keep all limbs clear of pinch points and crush zones on ramps and trailers).
- Only exit customer sites when load is secure and agreed by site banksman or other representative (check if escort is required).
- If any part of the loading/unloading of plant falls outside of the above at any time or the operation does not go as planned STOP IMMEDIATELY and report to site foreman and Lynch Transport Office DO NOT CONTINUE until a recovery solution has been agreed by Lynch Transport Office and customer.

**FOR MORE INFORMATION, PLEASE REFER TO RISK ASSESSMENT 21**

# SSOW 05g: Loading/Unloading Trailer/Bowser - Transporters

Issue Number: 01

Date of Issue: 18/01/23

Based on: RA21

## PPE TO BE WORN

For this activity, the following PPE must be worn

- High-vis Vest / Jacket
- Safety Boots
- Safety Gloves
- Safety Helmet
- Safety Glasses
- (Site/Contract Specific)



## SAFE SYSTEM OF WORK

- All Lynch transport drivers must report to site security/office on arrival at any customer site and wait further instruction before continuing (Site Escort may be required).
- Once the driver has been directed to the plant loading/unloading area contact must be made with site Black/Grey Hat and confirm if a Point of Works Risk Assessment and Task Briefing is required prior to any loading/unloading activity can commencing.
- Always communicate with site staff to ensure all on-site risks have been identified, controlled and you have understood any information received.
- Only qualified (CPCS A49/A50/68 NPORS plant mover), competent and authorised personnel to load/unload plant on Lynch transport vehicles.
- All Lynch drivers are trained in working at height and refer to RA20 If required.
- Staff to follow safe loading procedures for each machine type in the Transport Safety Manual when loading/unloading.
- Drivers must ensure they have received familiarisation training for the item of plant (Trailer/Bowser) being loaded/unloaded. (Training records to be maintained).
- Lynch drivers must follow safe loading procedures for each machine type as documented in the Transport Safety Manual when loading/unloading plant.
- Ensure the transporter vehicle is parked on firm and level ground prior to any loading/unloading activity being carried out. (NEVER position the transporter vehicle facing up hill as this will increase the load bed entry angle).
- The driver will Maintain 3 points of contact when entering/exiting the vehicle cab at all times and when working near the edge of the load bed (when possible).
- The vehicle load bed and ramps will be kept free of debris, mud, oil, grease, waste, ice and other possible slip/trip hazards. (Sand to be spread where necessary to increase traction).
- Ensure vehicle/trailer jackleg/ramps (including neck ramps if required) are in place prior to all plant loading / unloading activities.
- Keep all limbs clear of pinch points and crush zones on ramps and trailers when setting load bed to receive Trailer/Bowser.
- In adverse weather conditions use sand/grit to increase traction.
- The driver must check that all bowzers are empty before any loading can commence, check with Black/Grey Hat where bowser can be discharged on site, if there is no provision to empty the bowser contact Lynch Transport Office for further instruction.
- Prior to loading any Trailer/Bowser from site ensure both plant items are free from any defects which could affect the safe loading. If in doubt STOP, do not attempt to move the Trailer/Bowser, inform the Black/Grey Hat and contact Lynch Transport Office for further instruction.
- The driver must ensure all service lines are attached and connected correctly to both trailer/bowser prior to moving the combination.

## SAFE SYSTEM OF WORK

- The driver must check tyre pressures on both Trailer/Bowser and trailer are correct, and a rolling brake check must be carried out in a controlled manner prior to any loading taking place. If in doubt STOP and contact Lynch Transport Office for further instruction.
- If loading a tractor with an attachment. DO NOT operate the attachment by use of the spool valve controls inside tractor cab.
- If loading/unloading a dual braked trailer/bowser, ensure that the brake pedals are locked together by locking bar to eliminate the brakes engaging independently of each other.
- Three points of contact to be maintained including the use of factory fitted handrails/holds and foot holds provided.
- Always follow the sites safe systems of work and routes provided when driving under overhead power cables.
- DO NOT work directly underneath overhead power cables.
- Do NOT work within 9 metres horizontally of overhead power cables supported on wooden poles.
- DO NOT work within 15 metres horizontally of overhead power cables supported on steel pylons or structures.
- Ensure all unauthorised personnel are kept a safe distance from plant/transporter loading/unloading. Use banksman where possible to control pedestrian/plant interface.
- Banksman to remain a safe distance (outside red zone) during loading/unloading activities. (If the driver loses sight of the banksman the driver must STOP until contact has been re-established).
- To reduce drivers/operators being injured in the event of a plant machine rolling over, seatbelts are worn at all times.
- All loading/unloading to be carried out safe, controlled manner - Do Not Rush.
- Tractor/trailer combination must be secured to transporters as per manufacturer's guidance and Transport Safety Manual procedures.
- Never jump off the transporter load bed, walk down the ramps or sit down on load bed and lower yourself gently off the edge.
- Lashing chains, slings, timber, ropes etc. to be stowed safely when not in use so as not to create trip or falling hazards.
- After tractor/trailer combination is loaded and secured, jacklegs and ramps to be stowed (driver to keep all limbs clear of pinch points and crush zones on ramps and trailers).
- If any part of the loading/unloading of plant falls outside of the above at any time or the operation does not go as planned STOP IMMEDIATELY and report to site Black/Grey Hat and Lynch Transport Office DO NOT CONTINUE until a recovery solution has been agreed by Lynch Transport Office and customer.

## IDENTIFIED RISKS

The SSOW above has been compiled after identifying the following risks from this activity:

- Falls from height, people or objects from the transporter load bed and ramps
- Falls from height of vehicles drivers from the transporter load bed and ramps
- Slips & Trips (injuries sustained, sprains, abrasions, lacerations).
- Driver injuries from moving vehicle/trailer parts (ramps/jacklegs).
- Site personnel struck by moving vehicles and plant. (PPI).
- Electrocutation from contact with overhead power cables.

## LOADING/UNLOADING PROCEDURES

- Secure vehicle/trailer jacklegs for all plant loading / unloading activities.
- Set load bed for operation to take place; if the load projects 1/3 of the tractor/trailer combination set the transporter trailer to correct width as required by means of factory fitted greedy boards or widen trailer bed.
- If the load projects 305mm or width of 2.9m (but not more than 3.5m) fit red white markings (if at night fit lights).
- If a banksman is required, confirm lines of communication and ensure that the banksman stands in a safe position (outside the Red Zone) during all plant loading/unloading operations, if you lose sight of the banksman STOP until contact has been re-established.
- Establish an exclusion zone prior to all operations if site conditions/set up permits.
- Plant operator MUST hand over tractor/trailer control to the vehicle driver outside exclusion zone (plant operators are not permitted to load any plant onto a vehicle load bed).
- Only the HGV / plant delivery driver with CPCS A49/A50/68 or NPORS plant mover competencies are permitted to load any plant onto vehicles/trailers.
- The driver will Select the correct gear / engine speed for the loading/unloading operation.
- All loading/unloading to be carried out safe, controlled manner - Do Not Rush.
- Confirm (visual check) that the machine is as central to the load bed ramps as possible (in the middle) and secure to load bed as per manufacturer's guidance and Transport Safety Manual procedures.
- Ensure all doors and windows are closed (if fitted) and loose items secured in the cab before transit
- After tractor/trailer are loaded and secured, jacklegs and ramps to be stowed (driver to keep all limbs clear of pinch points and crush zones on ramps and trailers).
- Only exit customer sites when load is secure and agreed by site banksman or other representative (check if escort is required).
- If any part of the loading/unloading of plant falls outside of the above at any time or the operation does not go as planned STOP IMMEDIATELY and report to site foreman and Lynch Transport Office DO NOT CONTINUE until a recovery solution has been agreed by Lynch Transport Office and customer.

**FOR MORE INFORMATION, PLEASE REFER TO RISK ASSESSMENT 21**

# SSOW 05h: Loading/Unloading Tracked Dumpers - Transporters

Issue Number: 01

Date of Issue: 18/01/23

Based on: RA21

## PPE TO BE WORN

- High-vis Vest / Jacket
- Safety Boots
- Safety Gloves
- Safety Helmet
- Safety Glasses
- (Site/Contract Specific)



For this activity, the following PPE must be worn

## SAFE SYSTEM OF WORK

- All Lynch transport drivers must report to site security/office on arrival at any customer site and wait further instruction before continuing (Site Escort may be required).
- Once the driver has been directed to the plant loading/unloading area contact must be made with site Black/Grey Hat and confirm if a Point of Works Risk Assessment and Task Briefing is required prior to any loading/unloading activity can commencing.
- Always communicate with site staff to ensure all on-site risks have been identified, controlled and you have understood any information received.
- Only qualified (CPCS A49/A50/68 NPORS plant mover), competent and authorised personnel to load/unload plant on Lynch transport vehicles.
- All Lynch drivers are trained in working at height and refer to RA20 If required.
- Staff to follow safe loading procedures for each machine type in the Transport Safety Manual when loading/unloading.
- Drivers must ensure they have received familiarisation training for the tracked Dumper being loaded/unloaded. (Training records to be maintained).
- Lynch drivers must follow safe loading procedures for each machine type as documented in the Transport Safety Manual when loading/unloading plant.
- Ensure the transporter vehicle is parked on firm and level ground prior to any loading/unloading activity being carried out.
- The driver will Maintain 3 points of contact when entering/exiting the vehicle cab at all times and when working near the edge of the load bed (when possible).
- The vehicle load bed and ramps will be kept free of debris, mud, oil, grease, waste, ice and other possible slip/trip hazards. (Sand to be spread where necessary to increase traction).
- Ensure vehicle/trailer jackleg/ramps (including neck ramps if required) are in place prior to all plant loading / unloading activities.
- Keep all limbs clear of pinch points and crush zones on ramps and trailers when setting load bed to receive tracked dumpers.
- In adverse weather conditions use sand/grit to increase traction.
- Prior to loading any tracked Dumper from site ensure that plant items are free from any defects which could affect the safe loading. If in doubt STOP, do not attempt to move the tracked Dumper, inform the Black/Grey Hat and contact Lynch Transport Office for further instruction.
- Checks should be made to the tracked dumper before operation takes place to ensure track tension is correct and that both tracks are operational, if in doubt STOP and contact Lynch Transport Office for further instruction.

## SAFE SYSTEM OF WORK

- Three points of contact to be maintained including the use of factory fitted handrails/holds and foot holds provided.
- Always follow the sites safe systems of work and routes provided when driving under overhead power cables.
- DO NOT work directly underneath overhead power cables.
- Do NOT work within 9 metres horizontally of overhead power cables supported on wooden poles.
- DO NOT work within 15 metres horizontally of overhead power cables supported on steel pylons or structures.
- Ensure all unauthorised personnel are kept a safe distance from plant/transporter loading/unloading. Use banksmen where possible to control pedestrian/plant interface.
- Banksman to remain a safe distance (outside red zone) during loading/unloading activities. (If the driver loses sight of the banksman the driver must STOP until contact has been re-established).
- To reduce drivers/operators being injured in the event of a plant machine rolling over, seatbelts are worn at all times.
- All loading/unloading to be carried out safe, controlled manner - Do Not Rush.
- The tracked dumper must be secured to transporters as per manufacturer's guidance and Transport Safety Manual procedures.
- Never jump off the transporter load bed, walk down the ramps or sit down on load bed and lower yourself gently off the edge.
- Lashing chains, slings, timber, ropes etc. to be stowed safely when not in use so as not to create trip or falling hazards.
- After the tracked dumper is loaded and secured, jacklegs and ramps to be stowed (driver to keep all limbs clear of pinch points and crush zones on ramps and trailers).
- If any part of the loading/unloading of plant falls outside of the above at any time or the operation does not go as planned STOP IMMEDIATELY and report to site Black/Grey Hat and Lynch Transport Office DO NOT CONTINUE until a recovery solution has been agreed by Lynch Transport Office and customer.

## IDENTIFIED RISKS

The SSOW above has been compiled after identifying the following risks from this activity:

- Falls from height, people or objects from the transporter load bed and ramps
- Falls from height of vehicles drivers from the transporter load bed and ramps
- Slips & Trips (injuries sustained, sprains, abrasions, lacerations).
- Driver injuries from moving vehicle/trailer parts (ramps/jacklegs.
- Site personnel struck by moving vehicles and plant. (PPI).
- Electrocution from contact with overhead power cables.

# SSOW 05h: Loading/Unloading Tracked Dumpers - Transporters

Issue Number: 01

Date of Issue: 18/01/23

Based on: RA21

## LOADING/UNLOADING PROCEDURES

- Secure vehicle/trailer jacklegs for all plant loading / unloading activities.
- Set load bed for operation to take place; if the load projects 1/3 of the tracked dumper set trailer to correct width as required by means of factory fitted greedy boards or widen trailer bed.
- If the load projects 305mm or width of 2.9m (but not more than 3.5m) fit red white markings (if at night fit lights).
- If a banksman is required, confirm lines of communication and ensure that the banksman stands in a safe position (outside the Red Zone) during all plant loading/unloading operations, if you lose sight of the banksman STOP until contact has been re-established.
- Establish an exclusion zone prior to all operations if site conditions/set up permits.
- Plant operator MUST hand over the tracked dumper to the vehicle driver outside exclusion zone (plant operators are not permitted to load any plant onto a vehicle load bed).
- Only the HGV / plant delivery driver with CPCS A49/A50/68 or NPORS plant mover competencies are permitted to load any plant onto vehicles/trailers.
- The driver will Select the correct gear / engine speed for the loading/unloading operation.
- All loading/unloading to be carried out safe, controlled manner - Do Not Rush.
- Confirm (visual check) that the machine is as central to the load bed ramps as possible (in the middle) and secure to load bed as per manufacturer's guidance and Transport Safety Manual procedures.
- Ensure all doors and windows are closed (if fitted) and loose items secured in the cab before transit
- After the tracked dumper is loaded and secured, jacklegs and ramps to be stowed (driver to keep all limbs clear of pinch points and crush zones on ramps and trailers).
- Only exit customer sites when load is secure and agreed by site banksman or other representative (check if escort is required).
- If any part of the loading/unloading of plant falls outside of the above at any time or the operation does not go as planned STOP IMMEDIATELY and report to site foreman and Lynch Transport Office DO NOT CONTINUE until a recovery solution has been agreed by Lynch Transport Office and customer.

**FOR MORE INFORMATION, PLEASE REFER TO RISK ASSESSMENT 21**

# SSOW 05i: Loading/Unloading Bulldozers - Transporters

Issue Number: 01

Date of Issue: 18/01/23

Based on: RA21

## PPE TO BE WORN

For this activity, the following PPE must be worn

- High-vis Vest / Jacket
- Safety Boots
- Safety Gloves
- Safety Helmet
- Safety Glasses
- (Site/Contract Specific)



## SAFE SYSTEM OF WORK

- All Lynch transport drivers must report to site security/office on arrival at any customer site and wait further instruction before continuing (Site Escort may be required).
- Once the driver has been directed to the plant loading/unloading area contact must be made with site Black/Grey Hat and confirm if a Point of Works Risk Assessment and Task Briefing is required prior to any loading/unloading activity can commencing.
- Always communicate with site staff to ensure all on-site risks have been identified, controlled and you have understood any information received.
- Only qualified (CPCS A49/A50/68 NPORS plant mover), competent and authorised personnel to load/unload plant on Lynch transport vehicles.
- All Lynch drivers are trained in working at height and refer to RA20 If required.
- Staff to follow safe loading procedures for each machine type in the Transport Safety Manual when loading/unloading.
- Drivers must ensure they have received familiarisation training for the Bulldozer being loaded/unloaded. (Training records to be maintained).
- Lynch drivers must follow safe loading procedures for each machine type as documented in the Transport Safety Manual when loading/unloading plant.
- Ensure the transporter vehicle is parked on firm and level ground prior to any loading/unloading activity being carried out.
- The driver will Maintain 3 points of contact when entering/exiting the vehicle cab at all times and when working near the edge of the load bed (when possible).
- The vehicle load bed and ramps will be kept free of debris, mud, oil, grease, waste, ice and other possible slip/trip hazards. (Sand to be spread where necessary to increase traction).
- Ensure vehicle/trailer jackleg/ramps (including neck ramps if required) are in place prior to all plant loading / unloading activities.
- Keep all limbs clear of pinch points and crush zones on ramps and trailers when setting load bed to receive item of plant.
- In adverse weather conditions use sand/grit to increase traction.
- Prior to loading any Bulldozer from site ensure that plant items are free from any defects which could affect the safe loading. If in doubt STOP, do not attempt to move the Bulldozer, inform the Black/Grey Hat and contact Lynch Transport Office for further instruction.
- Checks should be made to the Bulldozer before operation takes place to ensure track tension is correct and that both tracks are operational If in doubt STOP and contact Lynch Transport Office for further instruction.

## SAFE SYSTEM OF WORK

- Three points of contact to be maintained including the use of factory fitted handrails/holds and foot holds provided.
- Always follow the sites safe systems of work and routes provided when driving under overhead power cables.
- DO NOT work directly underneath overhead power cables.
- Do NOT work within 9 metres horizontally of overhead power cables supported on wooden poles.
- DO NOT work within 15 metres horizontally of overhead power cables supported on steel pylons or structures.
- Ensure all unauthorised personnel are kept a safe distance from plant/transporter loading/unloading. Use banksmen where possible to control pedestrian/plant interface.
- Banksman to remain a safe distance (outside red zone) during loading/unloading activities. (If the driver loses sight of the banksman the driver must STOP until contact has been re-established).
- To reduce drivers/operators being injured in the event of a plant machine rolling over, seatbelts are worn at all times.
- All loading/unloading to be carried out safe, controlled manner – Do Not Rush.
- The Bulldozer must be secured to transporters as per manufacturer's guidance and Transport Safety Manual procedures.
- Never jump off the transporter load bed, walk down the ramps or sit down on load bed and lower yourself gently off the edge.
- Lashing chains, slings, timber, ropes etc. to be stowed safely when not in use so as not to create trip or falling hazards.
- After the Bulldozer is loaded and secured, jacklegs and ramps to be stowed (driver to keep all limbs clear of pinch points and crush zones on ramps and trailers).
- If any part of the loading/unloading of plant falls outside of the above at any time or the operation does not go as planned STOP IMMEDIATELY and report to site Black/Grey Hat and Lynch Transport Office DO NOT CONTINUE until a recovery solution has been agreed by Lynch Transport Office and customer.

## IDENTIFIED RISKS

The SSOW above has been compiled after identifying the following risks from this activity:

- Falls from height, people or objects from the transporter load bed and ramps
- Falls from height of vehicles drivers from the transporter load bed and ramps
- Slips & Trips (injuries sustained, sprains, abrasions, lacerations).
- Driver injuries from moving vehicle/trailer parts (ramps/jacklegs).
- Site personnel struck by moving vehicles and plant. (PPI).
- Electrocutation from contact with overhead power cables.

## LOADING/UNLOADING PROCEDURES

- Secure vehicle/trailer jacklegs for all plant loading / unloading activities.
- Set load bed for operation to take place; if the load projects 1/3 of the bulldozer set trailer to correct width as required by means of factory fitted greedy boards or widen trailer bed.
- If the load projects 305mm or width of 2.9m (but not more than 3.5m) fit red white markings (if at night fit lights).
- If a banksman is required, confirm lines of communication and ensure that the banksman stands in a safe position (outside the Red Zone) during all plant loading/unloading operations, if you lose sight of the banksman STOP until contact has been re-established.
- Establish an exclusion zone prior to all operations if site conditions/set up permits.
- Plant operator MUST hand over the Bulldozer to the vehicle driver outside exclusion zone (plant operators are not permitted to load any plant onto a vehicle load bed).
- Only the HGV / plant delivery driver with CPCS A49/A50/68 or NPORS plant mover competencies are permitted to load any plant onto vehicles/trailers.
- The driver will Select the correct gear / engine speed for the loading/unloading operation.
- All loading/unloading to be carried out safe, controlled manner – Do Not Rush.
- When possible reverse Bull dozers onto the load bed to ensure best visibility when moving the machine on the public highway
- Confirm (visual check) that the machine is as central to the load bed ramps as possible (in the middle) and secure to load bed as per manufacturer's guidance and Transport Safety Manual procedures.
- Ensure all doors and windows are closed (if fitted) and loose items secured in the cab before transit
- After the Bulldozer is loaded and secured, jacklegs and ramps to be stowed (driver to keep all limbs clear of pinch points and crush zones on ramps and trailers).
- Only exit customer sites when load is secure and agreed by site banksman or other representative (check if escort is required).
- If any part of the loading/unloading of plant falls outside of the above at any time or the operation does not go as planned STOP IMMEDIATELY and report to site foreman and Lynch Transport Office DO NOT CONTINUE until a recovery solution has been agreed by Lynch Transport Office and customer.

**FOR MORE INFORMATION, PLEASE REFER TO RISK ASSESSMENT 21**

# SSOW 05j: Loading/Unloading Telehandlers - Transporters

Issue Number: 01

Date of Issue: 18/01/23

Based on: RA21

## PPE TO BE WORN

- High-vis Vest / Jacket
- Safety Boots
- Safety Gloves
- Safety Helmet
- Safety Glasses
- (Site/Contract Specific)



For this activity, the following PPE must be worn

## SAFE SYSTEM OF WORK

- All Lynch transport drivers must report to site security/office on arrival at any customer site and wait further instruction before continuing (Site Escort may be required).
- Once the driver has been directed to the plant loading/unloading area contact must be made with site Black/Grey Hat and confirm if a Point of Works Risk Assessment and Task Briefing is required prior to any loading/unloading activity can commencing.
- Always communicate with site staff to ensure all on-site risks have been identified, controlled and you have understood any information received.
- Only qualified (CPCS A49/A50/68 NPORS plant mover), competent and authorised personnel to load/unload plant on Lynch transport vehicles.
- All Lynch drivers are trained in working at height and refer to RA20 If required.
- Staff to follow safe loading procedures for each machine type in the Transport Safety Manual when loading/unloading.
- Drivers must ensure they have received familiarisation training for the item of plant (Telehandler) being loaded/unloaded. (Training records to be maintained).
- Lynch drivers must follow safe loading procedures for each machine type as documented in the Transport Safety Manual when loading/unloading plant.
- Ensure the transporter vehicle is parked on firm and level ground prior to any loading/unloading activity being carried out.
- The driver will Maintain 3 points of contact when entering/exiting the vehicle cab at all times and when working near the edge of the load bed (when possible).
- The vehicle load bed and ramps will be kept free of debris, mud, oil, grease, waste, ice and other possible slip/trip hazards. (Sand to be spread where necessary to increase traction).
- Ensure vehicle/trailer jackleg/ramps (including neck ramps if required) are in place prior to all plant loading / unloading activities.
- Keep all limbs clear of pinch points and crush zones on ramps and trailers when setting load bed to receive Trailer/Bowser.
- In adverse weather conditions use sand/grit to increase traction.
- Prior to loading any Telehandler from site ensure that plant items are free from any defects which could affect the safe loading. If in doubt STOP, do not attempt to move the Telehandler, inform the Black/Grey Hat and contact Lynch Transport Office for further instruction.
- The driver must check tyre pressures on the telehandler, and a rolling brake check must be carried out in a controlled manner prior to any loading taking place. If in doubt STOP and contact Lynch Transport Office for further instruction.

## SAFE SYSTEM OF WORK

- Ensure that the telehandler is in two-wheel steer and not all wheel steer or crab steer before the operation takes place.
- Three points of contact to be maintained including the use of factory fitted handrails/holds and foot holds provided.
- Always follow the sites safe systems of work and routes provided when driving under overhead power cables.
- DO NOT work directly underneath overhead power cables.
- Do NOT work within 9 metres horizontally of overhead power cables supported on wooden poles.
- DO NOT work within 15 metres horizontally of overhead power cables supported on steel pylons or structures.
- Ensure all unauthorised personnel are kept a safe distance from plant/transporter loading/unloading. Use banksmen where possible to control pedestrian/plant interface.
- Banksman to remain a safe distance (outside red zone) during loading/unloading activities. (If the driver loses sight of the banksman the driver must STOP until contact has been re-established).
- To reduce drivers/operators being injured in the event of a plant machine rolling over, seatbelts are worn at all times.
- Telehandlers do not have weight distributed evenly due to the boom being to one side; keep the boom as low as possible during the operation.
- All loading/unloading to be carried out safe, controlled manner - Do Not Rush.
- Telehandler must be secured to transporters as per manufacturer's guidance and Transport Safety Manual procedures.
- Never jump off the transporter load bed, walk down the ramps or sit down on load bed and lower yourself gently off the edge.
- Lashing chains, slings, timber, ropes etc. to be stowed safely when not in use so as not to create trip or falling hazards.
- After the telehandler is loaded and secured, jacklegs and ramps to be stowed (driver to keep all limbs clear of pinch points and crush zones on ramps and trailers).
- If any part of the loading/unloading of plant falls outside of the above at any time or the operation does not go as planned STOP IMMEDIATELY and report to site Black/Grey Hat and Lynch Transport Office DO NOT CONTINUE until a recovery solution has been agreed by Lynch Transport Office and customer.

## IDENTIFIED RISKS

The SSOW above has been compiled after identifying the following risks from this activity:

- Falls from height, people or objects from the transporter load bed and ramps
- Falls from height of vehicles drivers from the transporter load bed and ramps
- Slips & Trips (injuries sustained, sprains, abrasions, lacerations).
- Driver injuries from moving vehicle/trailer parts (ramps/jacklegs).
- Site personnel struck by moving vehicles and plant. (PPI).
- Electrocution from contact with overhead power cables.

## ➤ **LOADING/UNLOADING PROCEDURES**

- Secure vehicle/trailer jacklegs for all plant loading / unloading activities.
- Set load bed for operation to take place; if the load projects 1/3 of the telehandler set the transporter trailer to correct width as required by means of factory fitted greedy boards or widen trailer bed.
- If the load projects 305mm or width of 2.9m (but not more than 3.5m) fit red white markings (if at night fit lights).
- If a banksman is required, confirm lines of communication and ensure that the banksman stands in a safe position (outside the Red Zone) during all plant loading/unloading operations, if you lose sight of the banksman STOP until contact has been re-established.
- Establish an exclusion zone prior to all operations if site conditions/set up permits.
- Plant operator MUST hand over telehandler control to the vehicle driver outside exclusion zone (plant operators are not permitted to load any plant onto a vehicle load bed).
- Only the HGV / plant delivery driver with CPCS A49/A50/68 or NPORS plant mover competencies are permitted to load any plant onto vehicles/trailers.
- The driver will Select the correct gear / engine speed for the loading/unloading operation.
- All loading/unloading to be carried out safe, controlled manner – Do Not Rush.
- Confirm (visual check) that the machine is as central to the load bed ramps as possible (in the middle) and secure to load bed as per manufacturer's guidance and Transport Safety Manual procedures.
- Ensure all doors and windows are closed (if fitted) and loose items secured in the cab before transit
- After the telehandler is loaded and secured, jacklegs and ramps to be stowed (driver to keep all limbs clear of pinch points and crush zones on ramps and trailers).
- Only exit customer sites when load is secure and agreed by site banksman or other representative (check if escort is required).
- If any part of the loading/unloading of plant falls outside of the above at any time or the operation does not go as planned STOP IMMEDIATELY and report to site foreman and Lynch Transport Office DO NOT CONTINUE until a recovery solution has been agreed by Lynch Transport Office and customer.

**FOR MORE INFORMATION, PLEASE REFER TO RISK ASSESSMENT 21**









Project	Workshop/Yard/Site Operations		RA No:	72	Review Date
Operation/Task	Ramp Operations - Transporters		Persons at risk	Employees	
Location/Area:	All Depots and Construction Sites/ Public Highway		Public at risk	Yes	

**KEY:** S = Severity Rating

1. Negligible
2. Minor Injury
3. Major Injury (RIDDOR)
4. Fatality
5. Multiple Fatality

**L = Likelihood of Occurrence**

1. Improbable
2. Remote 1 – 10%
3. Possible 10 – 50%
4. Probable 50 – 90%
5. Almost certain 90%+

**RR = Risk Rating**

Severity	5	5	10	15	20	25
	4	4	8	12	16	20
	3	3	6	9	12	15
	2	2	4	6	8	10
	1	1	2	3	4	5
	1	1	2	3	4	5



Unacceptable risk, plan out or add further controls  
 Acceptable only if no other method viable and with high level controls in place  
 Acceptable with suitable controls  
 Acceptable, no further action required

Activity	Hazards/Risks Identified	Risk Rating		Control Measures	Residual Risk			Responsibility	Monitoring Responsibility
		S	L		RR	S	L		
Transporter ramp operations	<ul style="list-style-type: none"> <li>Failure of ramp resulting in personal injury, property damage</li> <li>Crush &amp; entrapment hand/limb injuries</li> <li>Personal injuries from lowering/raising ramp</li> <li>Slips/trips</li> <li>Electrocution from contact with overhead power cables</li> </ul>	3	4	12	3	2	6	Drivers Plant Operators Transport Compliance Manager Transport Managers	Transport Managers Transport Compliance Manager Customers site Supervisors



Project	Workshop/Yard/Site Operations		RA No:	73	Review Date
Operation/Task	Load Security - Transporters		Persons at risk	Employees	
Location/Area:	All Depots and Construction Sites / Public Highway		Public at risk	Yes	

**KEY:** S = Severity Rating

1. Negligible
2. Minor Injury
3. Major Injury (RIDDOR)
4. Fatality
5. Multiple Fatality

**L = Likelihood of Occurrence**

1. Improbable
2. Remote 1 – 10%
3. Possible 10 – 50%
4. Probable 50 – 90%
5. Almost certain 90%+

**RR = Risk Rating**

Severity	5	5	10	15	20	25
	4	4	8	12	16	20
	3	3	6	9	12	15
	2	2	4	6	8	10
	1	1	2	3	4	5
	1	1	2	3	4	5



Unacceptable risk, plan out or add further controls  
 Acceptable only if no other method viable and with high level controls in place  
 Acceptable with suitable controls  
 Acceptable, no further action required

Activity	Hazards/Risks Identified	Risk Rating		Control Measures	Residual Risk			Responsibility	Monitoring Responsibility
		S	L		RR	S	L		
Securing of loads onto load beds	<ul style="list-style-type: none"> <li>Failure of load restraints</li> <li>Falls of people or objects from the transporter load bed and ramps.</li> <li>Falls of vehicles from the transporter load bed and ramps.</li> <li>Slips &amp; Trips.</li> <li>Personnel being struck by moving or falling vehicles.</li> <li>Hand injuries</li> </ul>	4	4	16	4	2	8	Drivers Transport Compliance Manager Transport Managers	Drivers Transport Managers Transport Compliance Manager Customers site Supervisors



Project	Workshop/Yard/Site Operations	RA No:	73	Review Date
Operation/Task	Load Security - Transporters	Persons at risk	Employees	
Location/Area:	All Depots and Construction Sites / Public Highway	Public at risk	Yes	

**KEY:** S = Severity Rating

1. Negligible
2. Minor Injury
3. Major Injury (RIDDOR)
4. Fatality
5. Multiple Fatality

**L = Likelihood of Occurrence**

1. Improbable
2. Remote 1 – 10%
3. Possible 10 – 50%
4. Probable 50 – 90%
5. Almost certain 90%+

**RR = Risk Rating**

Severity	5	5	10	15	20	25
4	4	4	8	12	16	20
3	3	3	6	9	12	15
2	2	2	4	6	8	10
1	1	1	2	3	4	5
	1	2	3	4	5	

- R** Unacceptable risk, plan out or add further controls
- O** Acceptable only if no other method viable and with high level controls in place
- Y** Acceptable with suitable controls
- G** Acceptable, no further action required

**Likelihood**

Activity	Hazards/Risks Identified	Risk Rating			Control Measures	Residual Risk			Responsibility	Monitoring Responsibility
		S	L	RR		S	L	RR		
Securing of loads onto load beds	<ul style="list-style-type: none"> <li>Failure of load restraints</li> <li>Falls of people or objects from the transporter load bed and ramps.</li> <li>Falls of vehicles from the transporter load bed and ramps.</li> <li>Slips &amp; Trips.</li> <li>Personnel being struck by moving or falling vehicles.</li> <li>Hand injuries</li> </ul>	4	4	16	<p>Pre-use inspections – ratchets, ensure that they are free from damage and fully operational.</p> <ul style="list-style-type: none"> <li>Where defects are found, DO NOT use, quarantine/dispose of and replace as required.</li> <li>All load restraints are to be used in line with manufacturers' guidelines and instructions.</li> <li>A minimum of 4 direct lashings are to be applied to each plant item loaded.</li> <li>All transporters carry an adequate supply of lashings/chocks. Should additional be required, report to the Transport Manager.</li> <li>Where possible, wheeled plant must be choked / have positive contact to a bulkhead.</li> <li>Where tubes/poles are loaded, these must be choked.</li> <li>If in doubt, STOP and contact the Transport office.</li> </ul>	4	2	8	Drivers Transport Compliance Manager Transport Managers Customers site Supervisors	Drivers Transport Compliance Manager Transport Managers Customers site Supervisors



Project	Workshop/Yard/Haulage & Transport Operations	RA No:	25	Review
Operation/Task	Use of Hiab Lorries	Persons at risk	Drivers/Customers	
Location/Area:	Lynch Depots /Customer Sites/ Public Highways.	Public at risk	Yes	

**KEY:** S = Severity Rating

1. Negligible
2. Minor Injury
3. Major Injury (RIDDOR)
4. Fatality
5. Multiple Fatality

**L = Likelihood of Occurrence**

1. Improbable
2. Remote 1 – 10%
3. Possible 10 – 50%
4. Probable 50 – 90%
5. Almost certain 90%+

**RR = Risk Rating**

Severity	5	5	10	15	20	25
4	4	4	8	12	16	20
3	3	3	6	9	12	15
2	2	2	4	6	8	10
1	1	1	2	3	4	5
	1	2	3	4	5	

- R** Unacceptable risk, plan out or add further controls
- O** Acceptable only if no other method viable and with high level controls in place
- Y** Acceptable with suitable controls
- G** Acceptable, no further action required

**Likelihood**

Activity	Hazards/Risks Identified	Risk Rating			Control Measures	Residual Risk			Responsibility	Monitoring Responsibility
		S	L	RR		S	L	RR		
LIFTING WITH CRANE	<ul style="list-style-type: none"> <li>Overturn of vehicle during loading or unloading operations due to excessive loads or unstable ground.</li> <li>Crushing/collision between load, personnel, or site vehicles.</li> <li>Falls from height of operator or materials forming the load.</li> <li>Bridge strikes or striking overhead structures.</li> <li>Entrapment / crush injury by the retraction of the legs</li> </ul>	4	4	16	<ul style="list-style-type: none"> <li>All drivers to have a valid CPC card for lorry loaders, or other valid competency.</li> <li>All drivers must have a valid cat C LGV licence and valid Driver CPC Card.</li> <li>At all times the lorry crane should be operated in accordance with the manufacturers recommended safe procedures and within specified loads and limits</li> <li>Follow specific SSOW for lifting operations as per in Lynch Manual.</li> <li>Hi-ab vehicles are only to be operated when on firm level ground and in accordance with L Lynch safe system of work.</li> <li>When using crane the driver/operator must extend the stabilising legs at all times and place blocks/plates beneath if required.</li> <li>Barriers for exclusion zone to be provided by the venue/site.</li> <li>Work area around Hi-ab lorry will be cordoned off when using the crane to prevent unauthorised access. Stop work if anyone encroaches into the exclusion zone.</li> </ul>	4	2	8	Transport Haulage Manager Transport Supervisor Transport Compliance Manager Depot Manager. Workshop Manager Workshop Foreman H&S Director. Mobile Service Manager	Transport Compliance Manager Depot Manager Workshop Manager Workshop Foreman Operatives and Supervisors H&S Director and Team Fitters/Operators Site Supervisors Service Manager



Project	Workshop/Yard/Haulage & Transport Operations	RA No:	25	Review
Operation/Task	Use of Hiab Lorries	Persons at risk	Drivers/Customers	
Location/Area:	Lynch Depots /Customer Sites/ Public Highways.	Public at risk	Yes	

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1. Negligible
2. Minor Injury
3. Major Injury (RIDDOR)
4. Fatality
5. Multiple Fatality

**L = Likelihood of Occurrence**

1. Improbable
2. Remote 1 – 10%
3. Possible 10 – 50%
4. Probable 50 – 90%
5. Almost certain 90%+

**RR = Risk Rating**

Severity	5	4	3	2	1	5	4	3	2	1	20	16	12	8	5
	R	O	Y	G		R	O	Y	G		R	O	Y	G	

Unacceptable risk, plan out or add further controls  
 Acceptable only if no other method viable and with high level controls in place  
 Acceptable with suitable controls  
 Acceptable, no further action required

**Likelihood**

Activity	Hazards/Risks Identified	Risk Rating		Control Measures	Residual Risk			Responsibility	Monitoring Responsibility
		S	L		RR	S	L		
Use of Crane Lorries	Electrocution from contact with overhead cables  Overturn of vehicle during loading or tipping operations due to excessive loads or unstable ground.  Crushing/collision between load, personnel, or site vehicles  Falls from height of operator or materials forming the load.  Electrocution from contact with overhead cables  Bridge strikes or striking overhead structures.  Entrapment by the operator when retracting the legs.	4	4	16	4	2	8	Transport Haulage Manager Transport Supervisor Transport Compliance Manager Depot Manager. Workshop Manager H&S Director. Mobile Service Manager	Transport Compliance Manager Depot Manager Workshop Manager Workshop Foreman Operatives and Supervisors H&S Director and Team Fitters/Operators Site Supervisors Service Manager  Checks will be carried out by Lynch Plant Hires Compliance Manager on random selection.  This is to ensure all SSOW are followed correctly



Project	Winch Operations	RA No:	66	Review Date
Operation/Task	Use of Electric vehicle Winch	Persons at risk	Fitters	
Location/Area:	Workshop/Yard/Site	Public at risk	No	

**KEY: S = Severity Rating**

1. Negligible
2. Minor Injury
3. Major Injury (RIDDOR)
4. Fatality
5. Multiple Fatality

**L = Likelihood of Occurrence**

1. Improbable
2. Remote 1 – 10%
3. Possible 10 – 50%
4. Probable 50 – 90%
5. Almost certain 90%+

**RR = Risk Rating**

Severity	5	4	3	2	1	5	4	3	2	1	20	16	12	8	5
	R	O	Y	G		R	O	Y	G		R	O	Y	G	

Unacceptable risk, plan out or add further controls  
 Acceptable only if no other method viable and with high level controls in place  
 Acceptable with suitable controls  
 Acceptable, no further action required

**Likelihood**

Activity	Hazards/Risks Identified	Risk Rating		Control Measures	Residual Risk			Responsibility	Monitoring Responsibility
		S	L		RR	S	L		
Using Electric Vehicle Winch  General Safety Precautions	Moving part hazard, Unfamiliarity of winch, Entrapment, Crush, Slip, Lacerations, Sprains, Fracture	4	4	16	4	3	12	Depot Manager. Workshop Manager Transport Manager Fleet Director	Depot Manager Transport Workshop Manager Supervisors Fleet Director and Team.



Project	Winch Operations	RA No.:	66	Review Date
Operation/Task	Use of Electric vehicle Winch	Persons at risk	Fitters	
Location/Area:	Workshop/Yard/Site	Public at risk	No	

**KEY:** S = Severity Rating

1. Negligible
2. Minor Injury
3. Major Injury (RIDDOR)
4. Fatality
5. Multiple Fatality

**RR = Risk Rating**

Severity	5	4	3	2	1	5	4	3	2	1
	25	20	16	12	8	25	20	16	12	8
	20	16	12	8	4	15	12	8	4	3
	15	12	8	4	2	10	8	4	2	1
	10	8	4	2	1	5	4	2	1	1

Unacceptable risk, plan out or add further controls  
 Acceptable only if no other method viable and with high level controls in place  
 Acceptable with suitable controls  
 Acceptable, no further action required

**Likelihood**

Activity	Hazards/Risks Identified	Risk Rating		Control Measures	Residual Risk			Responsibility	Monitoring Responsibility
		S	RR		S	L	RR		
Cont. General Safety Precautions	Cont. Moving part hazard, Unfamiliarity of winch, Entrapment, Crush, Slip, Lacerations, Sprains, Fracture	4	16	Do not submerge in water. Wear heavy leather gloves when handling the wire rope. Never winch with less than five (5) wraps of wire rope around the drum.	4	3	12	Depot Manager. Workshop Manager Transport Manager Fleet Director	Depot Manager Transport Workshop Manager Supervisors Fleet Director and Team.
Operator	Untrained Inexperienced Impatience No Safety Equipment or Attire	4	16	Read and understand the manual. Keep a copy with you for reference If there is something you don't understand "Do not guess" seek assistance from experienced operators. Be patient and assess the load situation thoroughly Always use accredited and rated accessories e.g. hook chains etc. Wear safety attire always. Think safety first, middle and last.	4	3	12	Depot Manager. Workshop Manager Transport Manager Fleet Director	Depot Manager Transport Workshop Manager Supervisors Fleet Director and Team.
Wire Rope	Frayed - Breakage - steel splinters when rope is being handled. Kinked - Breakage - kink may catch on things i.e. The end of the tray etc. Severely weakens rope Over Size - Breakage - too big a rope wrapped around a small diameter drum will cause early fatigue.	4	16	Replace. Replace.  Replace. Always use recommended rope size. Seek advice from accredited wire rope manufacturer.	4	3	12	Depot Manager. Workshop Manager Transport Manager Fleet Director	Depot Manager Transport Workshop Manager Supervisors Fleet Director and Team.



Project	Winch Operations	RA No.:	66	Review Date
Operation/Task	Use of Electric vehicle Winch	Persons at risk	Fitters	
Location/Area:	Workshop/Yard/Site	Public at risk	No	

**KEY:** S = Severity Rating

1. Negligible
2. Minor Injury
3. Major Injury (RIDDOR)
4. Fatality
5. Multiple Fatality

**RR = Risk Rating**

Severity	5	4	3	2	1	5	4	3	2	1
	25	20	16	12	8	25	20	16	12	8
	20	16	12	8	4	15	12	8	4	3
	15	12	8	4	2	10	8	4	2	1
	10	8	4	2	1	5	4	2	1	1

Unacceptable risk, plan out or add further controls  
 Acceptable only if no other method viable and with high level controls in place  
 Acceptable with suitable controls  
 Acceptable, no further action required

**Likelihood**

Activity	Hazards/Risks Identified	Risk Rating		Control Measures	Residual Risk			Responsibility	Monitoring Responsibility
		S	RR		S	L	RR		
Cont. Wire Rope	Under Size - Load may winch on unevenly and fall. Breakage - To have a rope break under load can be catastrophic. It can whip in any direction causing injury or death and may allow your load to run free to cause further hazards.	4	16	Replace. Always use recommended rope size. Seek advice from accredited wire rope manufacturer. Replace. Never stand between the load and the winch always winch on level ground. Always wear safety PPE/equipment on hands, feet body and eyes especially.	4	3	12	Depot Manager. Workshop Manager Transport Manager Fleet Director	Depot Manager Transport Workshop Manager Supervisors Fleet Director and Team.
Winching Loads	Winching on uneven ground - Load may winch on unevenly and fall. Winching on a hill or slope - Winching uphill may put far more load on your equipment than it is rated for Winching rolling loads - Not preparing load for winching - The load may not be prepared for winching properly.	4	12	Always load on the most level ground you can find and within reason, load across the hill so your load will not roll away if it becomes dislodged. Winching downhill may allow the load to over-run the rope resulting in the bird nesting of your rope. (See "Rope consideration while winching" section Read and understand the manual. Keep a copy with you for reference.	4	2	8	Depot Manager. Workshop Manager Transport Manager Fleet Director	Depot Manager Transport Workshop Manager Supervisors Fleet Director and Team.



Project	Winch Operations	RA No:	66	Review Date
Operation/Task	Use of Electric vehicle Winch	Persons at risk	Fitters	
Location/Area:	Workshop/Yard/Site	Public at risk	No	

**KEY:** S = Severity Rating

1. Negligible
2. Minor Injury
3. Major Injury (RIDDDOR)
4. Fatality
5. Multiple Fatality

**L = Likelihood of Occurrence**

1. Improbable
2. Remote 1 – 10%
3. Possible 10 – 50%
4. Probable 50 – 90%
5. Almost certain 90%+

**RR = Risk Rating**

Severity	5	4	3	2	1	5	4	3	2	1	20	16	12	8	5
	R	O	Y	G		R	O	Y	G		R	O	Y	G	

Unacceptable risk, plan out or add further controls  
 Acceptable only if no other method viable and with high level controls in place  
 Acceptable with suitable controls  
 Acceptable, no further action required

**Likelihood**

Activity	Hazards/Risks Identified	Risk Rating		Control Measures	Residual Risk			Responsibility	Monitoring Responsibility
		S	L		RR	S	L		
Cont.	Cont. Shock loads - This type of load imposes a strain on the winch many times the actual weight of the load and can cause failure of the cable or of the winch.	4	3	12	4	2	8	Depot Manager. Workshop Manager Transport Manager Fleet Director	Depot Manager Transport Workshop Manager Supervisors Fleet Director and Team.
Winching Over a long Duration	Overheating of gear box - Breakdown of oil causing eventual failure. Excessive wear, shorter life span, eventual failure	4	3	12	4	2	8	Depot Manager. Workshop Manager Transport Manager Fleet Director	Depot Manager Transport Workshop Manager Supervisors Fleet Director and Team.
Side Pulling	Loads up the winch on an incorrect angle - Winch damage or failure. Severe wear and tear. Rope will try to jump cheek plates	4	3	12	4	2	8	Depot Manager. Workshop Manager Transport Manager Fleet Director	Depot Manager Transport Workshop Manager Supervisors Fleet Director and Team.
Winch Loads Bigger Than Recommended	Winch failure, rope breakage, and severe wear and tear - Personal injury or death. Loss of load. Damage to truck.	4	4	16	4	3	12	Depot Manager. Workshop Manager Transport Manager Fleet Director	Depot Manager Transport Workshop Manager Supervisors Fleet Director and Team.



Project	Winch Operations	RA No:	66	Review Date
Operation/Task	Use of Electric vehicle Winch	Persons at risk	Fitters	
Location/Area:	Workshop/Yard/Site	Public at risk	No	

**KEY:** S = Severity Rating

1. Negligible
2. Minor Injury
3. Major Injury (RIDDDOR)
4. Fatality
5. Multiple Fatality

**L = Likelihood of Occurrence**

1. Improbable
2. Remote 1 – 10%
3. Possible 10 – 50%
4. Probable 50 – 90%
5. Almost certain 90%+

**RR = Risk Rating**

Severity	5	4	3	2	1	5	4	3	2	1	20	16	12	8	5
	R	O	Y	G		R	O	Y	G		R	O	Y	G	

Unacceptable risk, plan out or add further controls  
 Acceptable only if no other method viable and with high level controls in place  
 Acceptable with suitable controls  
 Acceptable, no further action required

**Likelihood**

Activity	Hazards/Risks Identified	Risk Rating		Control Measures	Residual Risk			Responsibility	Monitoring Responsibility
		S	L		RR	S	L		
Suspended Loads	Don't stand under suspended loads - Serious injury or death may occur. Do not use a winch that is not fitted with a brake to lift loads. Cluttered work area. Maintain a clear working space. Rope and equipment failure. Impatience. Laziness. Incorrect positioning of lifting equipment - The load may fall	4	4	16	4	3	12	Depot Manager. Workshop Manager Transport Manager Fleet Director	Depot Manager Transport Workshop Manager Supervisors Fleet Director and Team.



Project	Workshop/Yard/Highway Operations	RA No:	19	Review Date
Operation/Task	Vehicle/Plant Movements in and around the workshop or yard	Persons at risk	Employees/Visitors	
Location/Area:	All Depots Yards/Workshops/Highway	Public at risk	No	

**KEY:** S = Severity Rating

1. Negligible
2. Minor Injury
3. Major Injury (RIDDDOR)
4. Fatality
5. Multiple Fatality

**L = Likelihood of Occurrence**

1. Improbable
2. Remote 1 – 10%
3. Possible 10 – 50%
4. Probable 50 – 90%
5. Almost certain 90%+

**RR = Risk Rating**

Severity	5	5	10	15	20	25
	4	4	8	12	16	20
	3	3	6	9	12	15
	2	2	4	6	8	10
	1	1	2	3	4	5
	1	2	3	4	5	



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 Acceptable, no further action required

**Likelihood**

Activity	Hazards/Risks Identified	Risk Rating		Control Measures	Residual Risk			Responsibility	Monitoring Responsibility
		S	L		RR	S	L		
Vehicle and plant movements in and around yard, workshop and highways.	Personnel being struck, crushed, or run over by moving vehicles or plant resulting in major injury or death.  Damage to vehicles resulting from collisions.	4	4	16	4	2	8	Depot Manager. Workshop Managers Foreman/supervisors H&S Director Transport Service Drivers Plant Operators Fitters	Depot Manager Workshop Manager Workshop Foreman Operatives and Supervisors H&S Director and Team Transport Service Manager Foreman/Supervisor Drivers Plant Operators Fitters



Project	Workshop/Yard/Highway Operations	RA No:	19	Review Date
Operation/Task	Vehicle/Plant Movements in and around the workshop or yard	Persons at risk	Employees/Visitors	
Location/Area:	All Depots Yards/Workshops/Highway	Public at risk	No	

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Severity	5	5	10	15	20	25
	4	4	8	12	16	20
	3	3	6	9	12	15
	2	2	4	6	8	10
	1	1	2	3	4	5
	1	2	3	4	5	



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**Likelihood**

Activity	Hazards/Risks Identified	Risk Rating		Control Measures	Residual Risk			Responsibility	Monitoring Responsibility
		S	L		RR	S	L		
Vehicle and plant movements in and around yard, workshop and highways.	Personnel being struck, crushed, or run over by moving vehicles or plant resulting in major injury or death.  Not wearing seatbelts in vehicles – legal requirement - reduce injury from vehicle impact. Adverse weather and speeding – risk of loss of control, impact with other vehicles and property.	4	4	16	4	2	8	Depot Manager. Workshop Managers Foreman/supervisors H&S Director Transport Service Drivers Plant Operators Fitters	Depot Manager Workshop Manager Workshop Foreman Operatives and Supervisors H&S Director and Team Transport Service Manager Foreman/Supervisor Drivers Plant Operators Fitters



Project	Workshop/Yard/Highway Operations	RA No:	19	Review Date
Operation/Task	Vehicle/Plant Movements in and around the workshop or yard	Persons at risk	Employees/Visitors	
Location/Area:	All Depots Yards/Workshops/Highway	Public at risk	No	

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Severity	5	5	10	15	20	25
4	4	4	8	12	16	20
3	3	3	6	9	12	15
2	2	2	4	6	8	10
1	1	1	2	3	4	5
	1	2	3	4	5	

**Likelihood**

**R** Unacceptable risk, plan out or add further controls  
**O** Acceptable only if no other method viable and with high level controls in place  
**Y** Acceptable with suitable controls  
**G** Acceptable, no further action required

Activity	Hazards/Risks Identified	Risk Rating		Control Measures	Residual Risk			Responsibility	Monitoring Responsibility
		S	L		RR	S	L		
Vehicle and plant movements in and around yard, workshop and highways	In-cab technology and driver distraction, loss of concentration, impact with property, pedestrians, collision with other vehicles.  Playing loud music / wearing of headphones loss of concentration, impact with property, pedestrians, collision with other vehicles.  Reading and writing loss of concentration, impact with property, pedestrians, collision with other vehicles.  Reaching / leaning over (away from normal driving position)	4	4	16	4	2	8	Depot Manager. Workshop Managers Foreman/supervisors H&S Director Transport Service Drivers Plant Operators Fitters	Depot Manager Workshop Manager Workshop Foreman Operatives and Supervisors H&S Director and Team Transport Service Manager Foreman/Supervisor Drivers Plant Operators Fitters



Project	Workshop/Yard/Highway Operations	RA No:	19	Review Date
Operation/Task	Vehicle/Plant Movements in and around the workshop or yard	Persons at risk	Employees/Visitors	
Location/Area:	All Depots Yards/Workshops/Highway	Public at risk	No	

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**RR = Risk Rating**

Severity	5	5	10	15	20	25
4	4	4	8	12	16	20
3	3	3	6	9	12	15
2	2	2	4	6	8	10
1	1	1	2	3	4	5
	1	2	3	4	5	

**Likelihood**

**R** Unacceptable risk, plan out or add further controls  
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Activity	Hazards/Risks Identified	Risk Rating		Control Measures	Residual Risk			Responsibility	Monitoring Responsibility
		S	L		RR	S	L		
Vehicle and plant movements in and around yard, workshop and highways	Operating/looking at non-essential vehicle controls, e.g. dash cameras,  Excessively concentrating on the use of 360-degree camera systems	4	4	16	4	2	8	Depot Manager. Workshop Managers Workshop Foreman Operatives and Supervisors H&S Director and Team Transport Service Manager Foreman/Supervisor Drivers Plant Operators Fitters	Depot Manager Workshop Manager Workshop Foreman Operatives and Supervisors H&S Director and Team Transport Service Manager Foreman/Supervisor Drivers Plant Operators Fitters



Project	Workshop/Yard/Site Operations	RA No:	20	Review Date
Operation/Task	Work at Height	Persons at risk	Employees	
Location/Area:	All Depots/Sites	Public at risk	No	

- KEY:**
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  - Minor Injury
  - Major Injury (RIDDDOR)
  - Fatality
  - Multiple Fatality
- L = Likelihood of Occurrence**
- Improbable
  - Remote 1 – 10%
  - Possible 10 – 50%
  - Probable 50 – 90%
  - Almost certain 90%+

**RR = Risk Rating**

Severity	5	5	10	15	20	25
4	4	4	8	12	16	20
3	3	3	6	9	12	15
2	2	2	4	6	8	10
1	1	1	2	3	4	5
	1	2	3	4	5	

**Likelihood**

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**O** Acceptable only if no other method viable and with high level controls in place  
**Y** Acceptable with suitable controls  
**G** Acceptable, no further action required

Activity	Hazards/Risks Identified	Risk Rating		Control Measures	Residual Risk			Responsibility	Monitoring Responsibility
		S	L		RR	S	L		
Work at Height	<p>Falls of people from height from lorry bed, plant machines or when working from mobile steps, towers, or ladders.</p> <p>Falls of objects or materials from height as above</p>	4	4	16	4	2	8	<p>Workshop Manager</p> <p>Workshop Foreman/supervisor</p> <p>H&amp;S Director</p> <p>Transport Service Manager</p> <p>Transport Manager</p> <p>Depot Managers</p>	<p>Depot Manager</p> <p>Workshop Manager</p> <p>Workshop Foreman</p> <p>Operatives and Supervisors</p> <p>H&amp;S Director and Team</p> <p>Transport Service Manager</p> <p>Transport Manager</p>



Project	Workshop/Yard/Site Operations	RA No:	20	Review Date
Operation/Task	Work at Height	Persons at risk	Employees	
Location/Area:	All Depots/Sites	Public at risk	No	

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  - Minor Injury
  - Major Injury (RIDDDOR)
  - Fatality
  - Multiple Fatality
- L = Likelihood of Occurrence**
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**RR = Risk Rating**

Severity	5	5	10	15	20	25
4	4	4	8	12	16	20
3	3	3	6	9	12	15
2	2	2	4	6	8	10
1	1	1	2	3	4	5
	1	2	3	4	5	

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Activity	Hazards/Risks Identified	Risk Rating		Control Measures	Residual Risk			Responsibility	Monitoring Responsibility
		S	L		RR	S	L		
Work at Height	<p>Falls of people from height from lorry bed, plant machines or when working from mobile steps, towers or ladders.</p> <p>Falls of objects or materials from height as above</p>	4	4	16	4	2	8	<p>Workshop Manager</p> <p>Workshop Foreman/supervisor</p> <p>H&amp;S Director</p> <p>Transport Service Manager</p> <p>Transport Manager</p> <p>Depot Managers</p>	<p>Depot Manager</p> <p>Workshop Manager</p> <p>Workshop Foreman</p> <p>Operatives and Supervisors</p> <p>H&amp;S Director and Team</p> <p>Transport Service Manager</p> <p>Transport Manager</p>



Project	Workshop/Yard/Site Operations	RA No:	14	Review Date
Operation/Task	Use of Quick Hitch Systems to change attachments on Plant	Persons at risk	Operators, Banksmen, Operatives	
Location/Area:	All Depots	Public at risk	Yes	

**KEY:**

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- Fatality
- Multiple Fatality

**L = Likelihood of Occurrence**

- Improbable
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**RR = Risk Rating**

Severity	5	5	10	15	20	25
	4	4	8	12	16	20
	3	3	6	9	12	15
	2	2	4	6	8	10
	1	1	2	3	4	5
			1	2	3	4
				1	2	3
					1	2
						1

**Likelihood**

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**O** Acceptable only if no other method viable and with high level controls in place  
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**G** Acceptable, no further action required

Activity	Hazards/Risks Identified	Risk Rating		Control Measures	Residual Risk			Monitoring Responsibility
		S	L		RR	S	L	
Use of Quick Hitch Systems to change attachments on Plant machines.	Machine dipper arm striking site worker Bucket / Attachment detaching from Quick Hitch and striking an operative Bucket "swing back" when Quick Hitch released Caught fingers / hand in Quick Hitch. Upper limb strains from disengaging Quick Hitch. Falls from cab. Unauthorised operation of plant while changing quick hitch.	4	3	12	4	2	8	Depot Manager Workshop Manager Workshop Foreman Operatives and Supervisors H&S Director and Team



Project	Workshop/Yard/Site Operations	RA No:	14	Review Date
Operation/Task	Use of Quick Hitch Systems to change attachments on Plant	Persons at risk	Operators, Banksmen, Operatives	
Location/Area:	All Depots	Public at risk	Yes	

**KEY:**

**S = Severity Rating**

- Negligible
- Minor Injury
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- Fatality
- Multiple Fatality

**L = Likelihood of Occurrence**

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**RR = Risk Rating**

Severity	5	5	10	15	20	25
	4	4	8	12	16	20
	3	3	6	9	12	15
	2	2	4	6	8	10
	1	1	2	3	4	5
			1	2	3	4
				1	2	3
					1	2
						1

**Likelihood**

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Activity	Hazards/Risks Identified	Risk Rating		Control Measures	Residual Risk			Monitoring Responsibility
		S	L		RR	S	L	
Use of Quick Hitch Systems to change attachments on Plant machines.	Machine dipper arm striking site worker Bucket / Attachment detaching from Quick Hitch and striking an operative Bucket "swing back" when Quick Hitch released Caught fingers / hand in Quick Hitch. Upper limb strains from disengaging Quick Hitch. Falls from cab. Unauthorised operation of plant while changing quick hitch	4	4	16	4	2	8	Depot Manager Workshop Manager Workshop Foreman Operatives and Supervisors H&S Director and Team



Project	Workshop/Yard Operations/Site	RA No:	09	Review Date
Operation/Task	Lifting Operations	Persons at risk	Operatives, Bystanders	
Location/Area:	All Depots/Sites	Public at risk	No	

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4. Fatality
5. Multiple Fatality

**L = Likelihood of Occurrence**

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2. Remote 1 – 10%
3. Possible 10 – 50%
4. Probable 50 – 90%
5. Almost certain 90%+

**RR = Risk Rating**

Severity	5	4	3	2	1	5	4	3	2	1
	25	20	16	12	8	25	20	16	12	8



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 Acceptable with suitable controls  
 Acceptable, no further action required

**Likelihood**

Activity	Hazards/Risks Identified	Risk Rating		Control Measures	Residual Risk			Responsibility	Monitoring Responsibility
		S	L		RR	S	L		
Mechanical lifting using forklifts or excavators	<p>Failure of lifting appliance</p> <p>Failure of lifting accessories</p> <p>Incorrect slinging techniques leading to loads or parts of loads dropping or slipping whilst being lifted.</p> <p>3<sup>rd</sup> party intrusion into dangerous areas during lifting</p> <p>Contact with overhead cables.</p> <p>Contact with structures.</p> <p>Personnel being struck or crushed by suspended loads</p>	4	4	16	4	2	8	Depot Manager. Workshop Manager Workshop Foreman H&S Director.	Depot Manager Workshop Manager Workshop Foreman Operatives and Supervisors H&S Director and Team



Project	Workshop/Yard Operations/Site	RA No:	09	Review Date
Operation/Task	Lifting Operations	Persons at risk	Operatives, Bystanders	
Location/Area:	All Depots/Sites	Public at risk	No	

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4. Fatality
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2. Remote 1 – 10%
3. Possible 10 – 50%
4. Probable 50 – 90%
5. Almost certain 90%+

**RR = Risk Rating**

Severity	5	4	3	2	1	5	4	3	2	1
	25	20	16	12	8	25	20	16	12	8



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**Likelihood**

Activity	Hazards/Risks Identified	Risk Rating		Control Measures	Residual Risk			Responsibility	Monitoring Responsibility
		S	L		RR	S	L		
Mechanical lifting using forklifts or excavators.	<p>Failure of lifting appliance</p> <p>Failure of lifting accessories</p> <p>Incorrect slinging techniques leading to loads or parts of loads dropping or slipping whilst being lifted.</p> <p>3<sup>rd</sup> party intrusion into dangerous areas during lifting</p> <p>Contact with overhead cables.</p> <p>Contact with structures.</p> <p>PPE</p>	4	4	16	4	2	8	Depot Manager. Workshop Manager Workshop Foreman H&S Director.	Depot Manager Workshop Manager Workshop Foreman Operatives and Supervisors H&S Director and Team

Guide to loads C&U, STGO

Tracked excavator	14t Excavator	20t Excavator	22t Excavator	30t Excavator	35t Excavator	40t Excavator	50t Excavator	70t Excavator	75t Excavator
32t Rigid Beavertail									
44/80t Artic Stepframe									
150t Lowbed (Dropneck)									

Dozer	D2 8.5t	D3 9.7t	D5 23t	D6 24t	D7 29t	D8 40t	D37PX 9.5t	D51PX 14t	D61PX 19.5t	D65PX 23t	D85PX 29t
32t Rigid Beavertail											
44/80t Artic Stepframe											
150t Lowbed (Dropneck)											

wheeled plant	Backhoe loader	12t Hydrema	18t Dumptruck	20t Dumptruck	30t Dumptruck	40t Dumptruck	Tractor	10t Duck	15t Duck	20t Duck
32t Rigid Beavertail										
44/80t Artic Stepframe										
150t Lowbed (Dropneck)										

Rollers	7t Roller	13t Roller	16t Roller	19t Roller
32t Rigid Beavertail				
44/80t Artic Stepframe				
150t Lowbed (Dropneck)				

Please note, the below will be loaded as a tractor trailer combination or transported separately depending on trailer dimensions

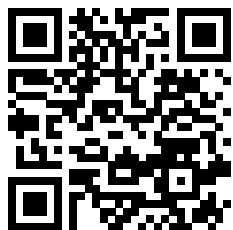
Tractor Towed	Dust Bowser	Fuel Bowser	Plant Trailer	Tipping Trailer	Flat bed trailer
32t Rigid Beavertail	Separate combination	combination	combination	combination	combination
44/80t Artic Stepframe					
150t Lowbed (Dropneck)					

32t Rigid Beavertail	A Lynch Rigid 32t Beavertail has max payload of 15t, Tare Weight 17t (Make / Model dependant)
44/80t Artic Stepframe	A Lynch 44t Stepframe tare weight is approx 27.5t, payload under C&U is 16.5t Approx. STGO Cat2 Payload 50t approx (2 Days Clear Movement order) (Make and Model Dependant)
150t Lowbed (Dropneck)	A Dropneck has a tare weight max of 50t Interdolley, 100t payload (5 clear Days Movement Order) - Escorted loads

C&U - Construction and Use Regulations 1986 - Lynch Vehicles within this band 32t Rigid, 44t Artic Stepframe for Transport operations  
 STGO - Special Types Goods Order 2003 - Lynch vehicles within band are 80t (Cat2), 150t (Cat 3)

Permitted  
 Not Permitted





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